

NORTHPOINT

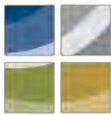
Somerville, Massachusetts

Application for Design Review – Parcel EF



Presented by:

DW NP Property, LLC
c/o DivcoWest
200 State Street, 12th Floor
Boston, MA 02109



BEALS + THOMAS

Prepared by:

Beals and Thomas, Inc.
Reservoir Corporate Center
144 Turnpike Road
Southborough, MA 01772

In collaboration with:

Tsoi Kobus Design
Michael Van Valkenburgh Associates, Inc.
Galluccio & Watson, LLP
Goulston & Storrs, PC

Submitted in Compliance with the City of Somerville Zoning Ordinance and M.G.L. c.40A

December 6, 2017



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December 6, 2017

Mr. Kevin Prior, Chair
Somerville Planning Board
City Hall
93 Highland Avenue
Somerville, MA 02143

Via: Hand Delivery

Reference: NorthPoint Parcel EF Design and Site Plan Review
Somerville, Massachusetts
B+T Project No. 2084.02

Dear Chairman Prior and Members of the Board:

On behalf of the Applicant, DW NP Property, LLC (an affiliate of DivcoWest), Beals and Thomas, Inc. respectfully submits this Design and Site Plan Review (DSP) Application for Parcel EF (the Site), which is part of the larger NorthPoint development partially located in Somerville, Boston, and Cambridge, Massachusetts. The proposed development is the construction of a ±483,000 gross square foot (above ground) commercial building. This filing is intended to replace the previous filing, having been submitted to the City on October 5, 2017.

As shown on the master plan for NorthPoint, the Site is bounded by Dawes Street to the south, an access driveway to the west, the NorthPoint site boundary to the north, and a new open space to the east. The Application herein is submitted in accordance with Section 5.4 of the City of Somerville Zoning Ordinance (SZO).

As part of the DSP Application, we have submitted the following materials for review by the Somerville Planning Board:

- Completed Application for Design and Site Plan Submission;
- Legal Description of the Property;
- Property Title Report;
- Property Deed;
- Locus Map;
- Project Description;
- Design Review Package, including building renderings, floor plans, elevations, materials palette,
- Environmental Analysis, and other required information;
- Site Plans;
- Landscaping Plans;
- Transportation Analysis Memorandum;
- Infrastructure Analysis; and
- Construction Plan.

CONTACT INFORMATION OF THE DEVELOPMENT TEAM

Owner/Applicant:

DW NP Property, LLC c/o DivcoWest
200 State Street, 12th Floor
Boston, MA 02109
Tom Sullivan, President, Development
Division
617-720-7400
tsullivan@divcowest.com

Architect:

Tsoi Kobus & Associates, Inc.
Scott Brown, Architect
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Boston, MA 02109
(617) 475 - 4000
sbrown@tsoikobus.design

Landscape Architect:

Michael Van Valkenburgh Associates, Inc.
231 Concord Avenue
Cambridge, MA 02138
Chris Matthews, Associate Principal
(617) 864-2076
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Civil Engineer:

Beals and Thomas, Inc.
144 Turnpike Road
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Legal Counsel:

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Darren M. Baird
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Legal Counsel:

Galluccio & Watson, LLP
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anthonygalluccio@comcast.net

Traffic Engineer:

Vanasse Hangen Brustlin, Inc.
99 High Street, 10th Floor
Boston, MA 02110
Susan P. Sloan-Rossiter, Principal
(617) 607-2930
ssloanrossiter@vhb.com

LEGAL DESCRIPTION OF PROPERTY

Three certain parcels of land in the Commonwealth of Massachusetts, County of Middlesex, City of Somerville, situated on the southeasterly boundary of the City of Somerville common with the City of Cambridge and shown in part on a plan recorded at the Middlesex County Registry of Deeds as plan 937 of 2012. More particularly bounded and described as follows:

First Parcel

Beginning at a point at the most northeasterly corner of the parcel herein described, thence running;

Westerly 1026 feet more or less by the city boundary between the City of Somerville and City of Cambridge, said course being by the centerline of the former Millers River channel, to a point, thence turning and running;

S 81° 14' 38" E 488 feet more or less to a point, thence turning and running;

S 77° 22' 25" E 264 feet more or less to the point of beginning, said last two courses being by land now or formerly of MBTA.

Containing 3.46 acres, more or less.

Second Parcel

Beginning at a point at the most northeasterly corner of the parcel herein described, said point being N 81° 14' 38" W 327 feet more or less from the most westerly corner of the first parcel, thence running;

Westerly 579 feet more or less by the city boundary between the City of Somerville and City of Cambridge, said course being by the centerline of the former Millers River channel, to a point, thence turning and running;

S 81° 14' 38" E 521 feet more or less to the point of beginning, said last course being by land now or formerly of MBTA.

Containing 0.87 acres, more or less.

Third Parcel

Beginning at a point at the most northeasterly corner of the parcel herein described, said point being N 81° 14' 38" W 258 feet more or less from the most westerly corner of the second parcel, thence running;

Westerly	317 feet more or less by the city boundary between the City of Somerville and City of Cambridge, said course being by the centerline of the former Millers River channel, to a point, thence turning and running;
N 02° 23' 37" W	28 feet more or less to a point, thence turning and running;
S 67° 40' 27" E	42.82 feet to a point, thence turning and running;
S 81° 14' 38" E	203 feet more or less to the point of beginning, said last three courses being by land now or formerly of MBTA.

Containing 0.37 acres, more or less.

PROJECT DESCRIPTION

The Site is currently undeveloped vacant former railroad land. It is one of twenty (20) building parcels in the NorthPoint mixed-use development. To date, condominium buildings on Lot S and Lot T, a rental residential building on Lot N, Northpoint Common, Child Street Park and related infrastructure and other public amenities (including the Brian P. Murphy Memorial Staircase) have been constructed in NorthPoint. The surrounding roadway network is currently undergoing construction. The Site is located in the North Point Special District (NPSD).

The new Parcel EF building is an approximately 483,000 gross square foot (above ground) Commercial Building to be used for a combination of Commercial Office, R&D and/or Laboratory, and Retail and Consumer Services uses. The proposed building will be approximately 150 feet high (as calculated pursuant to the SZO), plus three levels of underground parking containing approximately 379 parking spaces. The Site is located in the middle of NorthPoint and will eventually be surrounded on three sides with other commercial and residential uses in the larger mixed-use development. Because of the master planned nature of NorthPoint, the Applicant has a vested interest in ensuring high quality development on all sides of the Site and can control the overall condition in NorthPoint over multiple parcels to create the desired urban streetscape.

There are no legal judgments, action, covenants, conditions, or restrictions that control the proposed development.

ZONING CONFORMANCE

The Site is located within the NPSD. The stated purpose of the NPSD is to “establish regulations supporting the development of a comprehensively planned neighborhood of mixed-use, moderate- to high-density development within easy walking distance to transit...” The proposed development provides a high-density, mixed-use building within easy walking distance to transit within a comprehensively planned neighborhood. As such, the development proposed for Parcel EF conforms with the purpose of the NPSD.

The design of the building and layout of the Site conforms to all requirements of the SZO as detailed in the attached application. Additional information regarding the Site's conformance with zoning is included in the attached Design Review Package.

TRANSPORTATION ANALYSIS

A transportation analysis memorandum, confirming Parcel EF's conformance to the previous approved transportation plan as part of the Neighborhood Development Plan is attached.

INFRASTRUCTURE ANALYSIS

The water, storm water drainage, sewer, energy, and electrical utility systems will be provided to the Site through Cambridge. The capacity and adequacy of these utility systems will be sufficiently upgraded as necessary to serve the Site. The Project will not use any of Somerville's municipal services. As the Site is currently undeveloped and vacant, there is not anticipated to be significant impacts or disruptions to vehicular or pedestrian circulation, or impacts affecting any civic or recreation space or streetscape improvements during construction.

The proposed development will include a stormwater management system designed to mitigate potential impacts the Project could have on the existing watershed. Stormwater controls have been proposed to provide water quality treatment, total suspended solids removal and promote groundwater recharge. The proposed system has been designed to comply to the maximum extent practical with:

- The 2008 Massachusetts Department of Environmental Protection (DEP) Stormwater Management Handbook, and
- City of Cambridge Wastewater and Stormwater Management Guidelines.

In accordance with the approved stormwater master plan for the NorthPoint, stormwater from Parcel EF will be directed south to NorthPoint Common, and will ultimately be conveyed to the Lechmere Canal. The proposed drainage system consists of deep sump hooded catch basins, proprietary water quality inlets, an irrigation cistern and subsurface infiltration system. All stormwater Best Management Practices (BMPs) were designed to treat a minimum of the first 1.0 inch of runoff generated by the on-site impervious areas. The BMPs have been designed to provide sediment and total phosphorus removal, and promote groundwater infiltration.

The stormwater master plan for Northpoint was permitted through the City of Cambridge, and largely constructed in 2007 along with Archstone North Point Phase I and NorthPoint buildings on Lots S and T. The system is designed to convey flow from the Archstone North Point and NorthPoint properties to a newly constructed outfall at the Lechmere Canal. It utilizes the centrally located NorthPoint Common for stormwater treatment. The Common includes a series of sediment forebays, swales, and Miller's Pond is a wet pond that provides sediment removal and nutrient removal.

Under proposed conditions roof runoff from the building proposed on Parcel EF will be directed to a 15,000-gallon cistern located within the adjacent where it can be reused to irrigate the green roof areas and the landscaped areas in the park. Overflow from the cistern will drain to a subsurface infiltration system within Baldwin Park, where it can recharge groundwater and nutrient removal can occur. Overflow from the subsurface infiltration system will drain to Miller's Pond, and finally be directed to the Lechmere Canal.

Non-structural good housekeeping measures such as, source control, pollution prevention measures, and regular maintenance of the on-site stormwater best management practices are outlined in the Site Owner's Manual. In compliance with the Total Maximum Daily Load (TMDL) issued by the Massachusetts Department of Environmental Protection for the Charles River, fertilizers, herbicides and pesticides that contain phosphorus are prohibited and cannot be used on-site.

In compliance with the City of Cambridge requirements, the proposed storm drain system was analyzed based on the 25-year storm event using EPA SWMMM. The system has been designed to maintain freeboard of 1.5-feet below the road grade during the 25-year design storm event.

The estimated sewage generation for Parcel EF is 56,680 gallons per day (GPD). The estimated electricity demand load is 9,547 kVA. The estimated water consumption is 170,208 GPD. The estimated gas load is 58,900 CFH.

DEVELOPMENT PLAN

A development plan for construction is attached to this application.

A construction management program is included as part of this application. The Construction and Site Activities Summary attached as part of this application was originally designed for the construction of the building on Parcel JK, however the construction of the building on Parcel EF will follow the timeline and guidelines set forth in this document.

Mr. Kevin Prior, Chair
Somerville Planning Board
December 6, 2017
Page 7

We understand that the Planning Director will provide the requisite notice to Parties in Interest advertising the public hearing associated with this request.

We have previously included a check made out to the City of Somerville in the amount of \$8,500.00 (maximum fee) as required for the filing fees associated with Design and Site Plan Review. Also included in the previous filing were the \$350.00 Advertising and Notice Fee and \$25.00 Abutters List Fee.

We look forward to meeting with the Planning Board to review and discuss the proposed project. Thank you for your consideration of this application.

Very truly yours,

BEALS AND THOMAS, INC.



John P. Gelcich, AICP
Senior Planner

Enclosures

JPG/aak/208402PT028



CITY OF SOMERVILLE MASSACHUSETTS

Joseph A. Curtatone, Mayor

Office of Strategic Planning and Community Development (OSPCD)
City Hall 3rd Floor, 93 Highland Avenue, Somerville, MA 02143
(617) 625-6600 ext. 2500

DESIGN AND SITE PLAN (DSP) SUBMITTAL • COVER PAGE

In accordance with Article 5 of the Somerville Zoning Ordinance, the undersigned hereby submits the attached design and site plan for review according to applicable procedures and provisions of the Somerville Zoning Ordinance. Submittal materials required by this form are included as part of this submittal package.

Project NorthPoint Parcel EF
Agent Name Mark Johnson Phone 617-914-8600 Email mjohnson@divcowest.com

OWNER

Name DW NP Property, LLC c/o DivcoWest Real Estate Services
Address 200 State Street, 12th Floor
Boston, MA 02109
Phone 617-720-7400 Email mcarp@divcowest.com

APPLICANT

Name DW NP Property, LLC c/o DivcoWest Real Estate Services
Address 200 State Street, 12th Floor
Boston, MA 02109
Phone 617-720-7400 Email mcarp@divcowest.com

As Owner, I make the following representations:

1. I hereby certify that I am the owner of the property identified on this application form.
2. I hereby certify that the applicant named on this application form has been authorized by me to apply to develop and/or use the property listed above for the purposes indicated in this application.
3. I hereby certify that the agent, engineer and/or architect listed on this application form have been authorized to represent this application before the Planning Staff, the Planning Board, and/or the Zoning Board of Appeals.
4. I will permit Planning Staff to conduct site visits on my property.
5. Should the ownership of this parcel change before the board(s) have acted on this application, I will provide updated information and new copies of this signature page.

DW NP Property, LLC by

Signature _____
Executive Vice President of its general partner

As Applicant, I make the following representations:

1. The information supplied on and with this application form is accurate to the best of my knowledge.
2. If the current use of the property is a nonconforming use, I will furnish proof to the satisfaction of the SPGA that the nonconforming use is legal.
3. I will make no changes to the approved project plans without the prior approval of the SPGA.
4. If the proposed project is subject to linkage (SZO Article 15), I will sign all documents required by the Planning Staff/SPGA governing the amount and the method of payment of the linkage fee.
5. I will return the notice sign or pay for its replacement.
6. I will pay the fees associated with this application, including but not limited to: advertising the case in the newspaper and mailing notices to abutters.
7. I hereby certify that the agent, engineer and/or architect listed on this application form have been authorized by me to represent me before the Planning Staff, the Planning Board and/or the Zoning Board of Appeals as it relates to the development and/or use of this property.

DW NP Property, LLC by

Signature _____
Executive Vice President of its general partner

CITY OF SOMERVILLE USE ONLY

CASE NUMBER _____
ZONING DISTRICT(S) _____ WARD/ALDERMAN _____
WAIVER(S)/SPECIAL PERMIT(S) REQUIRED? YES ☐ () NO ☐
VARIANCE REQUIRED? YES ☐ () NO ☐
FILING FEE _____ ADD FEE _____
PLANNING MEETING DATE _____ ENGINEERING MEETING DATE _____
DESIGN REVIEW DATE _____
HEARING DATE _____

CITY CLERK STAMP

Design and Site Plan Fee Schedule

	BASE FEE	ADDITIONAL FEE	MAX. FEE	AD FEE	ABUTTERS FEE
Review/Final Submittal	\$300	\$5.00 per linear foot of thoroughfare \$0.20 per square foot of civic or recreation space \$0.20 per gross square foot of non-commercial floor space \$100/dwelling unit	\$8,500	\$350	\$25
Revision, Major Amendment	\$225	\$5.00 per linear foot of thoroughfare \$0.20 per square foot of civic or recreation space \$0.20 per gross square foot of non-commercial floor space \$100/dwelling unit	\$8,500	\$350	\$25
Revision, Minor Amendment	\$150	n/a	n/a	n/a	n/a
Subdivision	\$400	\$100/lot	\$6,500	\$250	\$25
Lot Split, Lot Merger, or Lot Line Adjustment	\$400/action	n/a	\$400/action	n/a	n/a

OFFICE OF STRATEGIC PLANNING AND COMMUNITY DEVELOPMENT

City Hall 3rd Floor . 93 Highland Avenue . Somerville, MA 02143

617-625-6600 ext. 2500

M-W 8:30 AM - 4:30 PM, Th 8:30 AM - 7:30 PM, F 8:30 AM - 12:30 PM

DESIGN AND SITE PLAN (DSP) SUBMITTAL REQUIREMENTS

Applicants shall meet with the Office of Strategic Planning and Community Development and the Engineering Department prior to the submission of any design and site plan to ascertain applicable submittal requirements. A completed copy of this form shall be included with any design and site plan submittal package. All plans shall be prepared by a registered architect, landscape architect, land surveyor, or professional engineer, as applicable.

GENERAL DOCUMENT AND GRAPHIC STANDARDS

1. All required materials shall be submitted as follows:

	REVIEW SUBMITTAL	FINAL SUBMITTAL	FORMAT	DOCUMENT SIZE	PLAN SHEET SIZE
Physical Copies:	3	8	printed bound booklet	8.5x11	foldable 11"x 17" (plans not to scale)
Electronic Copies:	1	1	.pdf	8.5x11	24"x 36" min.
	1	1	.dwg	n/a	24"x 36" min.

2. Review copies of design and site plans shall be submitted to the City Clerk's office per §5.4.4.A of the Somerville Zoning Ordinance. Final copies shall be submitted to the Planning Division for distribution to the Planning Board no less than 20 days prior to the scheduled Public Hearing.
3. All plan drawings shall include a title block with the project name, plan issue date, sheet number, sheet title, registrant stamp, registrant contact information, scale, revision number and date, assessor's map-block-lot number(s), and 2"x 2" City Clerk stamp block.
4. All thoroughfares and other civic and recreation spaces shall be clearly labeled.
5. A north arrow and scale shall be provided on each map or plan.

REQUIRED FOR ALL DESIGN & SITE PLAN SUBMITTALS (in addition to Cover Page)

Written/Graphic Information

1. Contact information (company name, business address, telephone number, designated contact, & email address) for each member or firm of the development team, including legal representation and all project consultants
2. Legal description of property, including meets and bounds
3. Property title report including current ownership and purchase options of all parcels in the development site
4. Property deed
5. Identification of any existing legal judgments, actions, covenants, conditions, and/or restrictions that may control development, if applicable
6. A locus map

Required	Supplied	Received
<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

REQUIRED FOR THE SUBDIVISION OF LAND, LOT SPLITS, LOT MERGERS, OR LOT LINE ADJUSTMENTS

Plan Drawings

1. A forty (1" to 40') scale or larger subdivision plan prepared in accordance with the Deed Indexing Standards of the Commonwealth of Massachusetts, latest edition, including but not limited to the following:
 - a. Municipal boundaries
 - b. Lot lines
 - c. Abutting properties including legal address, ownership information, and deed book number/page, plan, and map references
 - d. Existing buildings, fences, and retaining walls

Required	Supplied	Received
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

REQUIRED FOR THOROUGHFARE DESIGN & SITE PLAN SUBMITTALS

Written/Graphic Information

1. Written narrative describing the project, including but not limited to the following:
 - a. Zoning conformance review, including satisfaction of the purpose and provisions for the specific district(s) where the thoroughfare is located
 - b. Identification of any requested waivers or variances, as required
 - c. Conformance review of city policy and/or other plans deemed appropriate by the Planning Board, as applicable
 - d. A development plan, setting forth the anticipated timing of construction and opening of the thoroughfare
 - e. A construction management program including, but not limited to, plans for construction vehicle access routes, on-site construction worker parking, designation of material storage methods and locations, and designation of construction hours

Required	Supplied	Received
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Plan Drawings

	Required	Supplied	Received
1. Key plan, as necessary	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
2. Plan legend	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
3. Vehicular lane and sidewalk width cross sections	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
4. Twenty (1" to 20') scale or larger drawings including, but not limited to, the following:			
a. General plan	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
b. Profile plan	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
c. Utility & drainage plan	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
d. Layout plan	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
e. Grading plan	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
f. Landscape/planting plan	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
g. Traffic & street lighting plan	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
5. Construction drawings including, but not limited to, the following:			
a. Pavement section detail	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
b. Sidewalk detail	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
c. Curb detail	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
d. Wheel chair ramp detail	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
e. Driveway/alley crossing detail	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
f. Crosswalk detail	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
g. Gutter inlet detail	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
h. Water/sewer			
i. Trench detail	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
ii. Catch basin detail	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
iii. Connection & tie-in detail	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
iv. Manhole detail	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
v. Hydrant detail	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
i. Public furniture			
i. Bench detail	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
ii. Trash receptacle detail	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
iii. Light pole(s) detail	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
iv. Light pole foundation detail	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
v. Bike rack detail	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
vi. Retaining wall/seat wall detail	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
vii. Bollard detail	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
j. Landscape			
i. Street tree detail	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
ii. Planting bed detail	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
iii. Shrub detail	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
iv. Ground cover detail	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
v. Fence detail	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
6. Traffic Management			
a. Signal plan	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
b. Sign summary sheet	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
c. Loop detector detail	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
d. Lighting wire diagram	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
7. Utility and slope cross sections	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

REQUIRED FOR CIVIC SPACE DESIGN & SITE PLAN SUBMITTALS

Written/Graphic Information

Required Supplied Received

- | | | | |
|----------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|--------------------------|--------------------------|--------------------------|
| 1. Written narrative describing the project, including but not limited to the following: | | | |
| a. Identification of the proposed space by type | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| b. Description of the intended built character guiding the design of the space | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| c. Zoning conformance review, including satisfaction of the purpose and provisions for the specific district(s) where the civic or recreation space is located | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| d. Identification of any requested special permits or variance, as required | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| e. Conformance review of city policy and/or other plans deemed appropriate by the Planning Board, as applicable | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| f. A development plan, setting forth the anticipated timing of construction and opening of the proposed space | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| g. A construction management program including, but not limited to, plans for construction vehicle access routes, on-site construction worker parking, designation of material storage methods and location, and designation of construction hours | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| 2. A shadow study illustrating shadows cast by surrounding buildings according to the City of Somerville's Illustrating Building Shadows requirements. | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |

Plan Drawings

Required Supplied Received

- | | | | |
|---------------------------------------------------------------------------------------------------|--------------------------|--------------------------|--------------------------|
| 1. Neighborhood civic space plan, if applicable, identifying the subject space within the plan | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| 2. Twenty (1" to 20') scale or larger plan drawings including, but not limited to, the following: | | | |
| a. Existing conditions | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| b. Site preparation/demolition plan | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| c. Materials plan | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| d. Layout plan | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| e. Paving plan | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| f. Grading/drainage plan | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| g. Utilities plan | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| h. Landscape/planting plan | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| i. Electrical & lighting plan | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| j. Irrigation plan | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| 3. Construction drawings including, but not limited to, the following: | | | |
| a. Paving surfaces detail | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| b. Curb and edges detail | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| c. Walls and fencing detail | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| d. Public furniture | | | |
| i. Seating detail | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| ii. Trash receptacle detail | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| iii. Light pole(s) detail | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| iv. Light pole foundation detail | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| v. Bike rack detail | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| vi. Bollard detail | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| e. Landscape | | | |
| i. Tree planting detail | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| ii. Planting bed detail | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| iii. Shrub detail | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| iv. Ground cover detail | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| 4. Illustrated plant list | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |

REQUIRED FOR LOT/BUILDING DESIGN & SITE PLAN SUBMITTALS

Written/Graphic Information

Required Supplied Received

- | | | | |
|-----------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|-------------------------------------|-------------------------------------|--------------------------|
| 1. Project description including, but not limited to, the following: | | | |
| a. Identification of the proposed building(s) by type, as applicable | <input checked="" type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| b. Description of design intent guiding the character of the lot or building | <input checked="" type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| c. Zoning conformance review, including satisfaction of the purpose and provisions for the specific district(s) where the lot or building is located | <input checked="" type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| d. Zoning conformance statistical summary, including the following: | | | |
| i. Facade build out | <input checked="" type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| ii. Floorplate of each floor (sq. ft.) | <input checked="" type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| iii. Building height (stories and feet) | <input checked="" type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| iv. Ground floor occupation | <input checked="" type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| v. First floor fenestration percentage | <input checked="" type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| vi. Total floor area by use category | <input checked="" type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| vii. Total dwelling unit count | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| viii. Total dwelling unit count by number of bedrooms | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| ix. Number of parking and loading spaces by type | <input checked="" type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| e. Identification of any requested special permits or variances, as required | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| f. Conformance review of city policy and/or other plans deemed appropriate by the Planning Board, as applicable | <input checked="" type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| g. A development plan, setting forth the anticipated timing of construction and occupancy of the proposed development | <input checked="" type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| h. A construction management program including, but not limited to, plans for construction vehicle access routes, on-site construction worker parking, designation of material storage methods and locations, and designation of construction hours | <input checked="" type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| 2. Pedestrian level perspective renderings or illustrations depicting the interface between the proposed building(s) frontage and the public realm (one per front lot line; pedestrians not included). | <input checked="" type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| 3. Description of how the design of the building's base (generally) and first floor (specifically) creates a positive interface between the proposed building(s) frontage and the public realm. | | | |
| 4. Renderings or illustrations depicting the proposed building(s) in context with surrounding structures (existing and proposed). | <input checked="" type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| 5. Building material identification, including color and texture | <input checked="" type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |

Site and Building Design

Plan Drawings

Required Supplied Received

- | | | | |
|--------------------------------------------------------------------------------------------------------------------|-------------------------------------|-------------------------------------|--------------------------|
| 1. Twenty (1" to 20') scale or larger plan drawings including, but not limited to, the following: | | | |
| a. Existing site plan, illustrating: | <input checked="" type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| i. Topography (2 ft. contour lines) | <input checked="" type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| ii. Lot lines | <input checked="" type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| iii. Existing buildings and structures, as applicable, including area, stories, principal use, and actual setbacks | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| iv. Location, caliper, and species of individual trees of 6-inch caliper or more, if applicable | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| v. Natural features and significant geologic formations | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| b. Proposed site plan, illustrating: | | | |
| i. Lot lines, setback lines, and built-to lines | <input checked="" type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| ii. Building footprint, including floor plate size | <input checked="" type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| iii. Continuous facade widths | <input checked="" type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| iv. Facade build out calculation | <input checked="" type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| v. Actual facade setback distances, at all differing points | <input checked="" type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| vi. Individual tenant spaces and lobbies, color differentiated by use | <input checked="" type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| vii. Frontage widths for tenant spaces and lobbies | <input checked="" type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| viii. Primary pedestrian entrances/exits for individual tenant spaces and lobbies | <input checked="" type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| ix. Outdoor amenity areas, including size | <input checked="" type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| c. Floor plans, illustrating: | | | |
| i. Total floor plate size for each floor | <input checked="" type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |

	Required	Supplied	Received
ii. Individual uses/spaces, color differentiated by use, for each floor (residential units should be color differentiated by bedroom count)	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
iii. Indoor and outdoor amenity areas, including size	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
d. Ground level landscape plan, including:			
i. Plant schedule	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
ii. Illustrated plant list	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
iii. Electrical and lighting plan	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
2. Building sections (one per front lot line), illustrating:			
a. Individual floors, color differentiated by use	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b. Height measurements, including the height of each story and building height in total	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
3. Facade/elevation renderings (all sides), illustrating:			
a. Vertical and horizontal articulation of each facade (in full)	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b. Fenestration pattern of each facade (in full), including solid to void analysis for each floor	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
c. Close-up of the building's base (one per front lot line)	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
4. Construction drawings including, but not limited to, the following:			
a. Pavement section detail	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b. Sidewalk detail	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
c. Curb detail	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
d. Wheel chair ramp detail	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
e. Driveway/alley crossing detail	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
f. Furnishings			
i. Bench detail	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
ii. Trash receptacle detail	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
iii. Light pole(s) and light pole foundation detail(s)	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
iv. Bike rack detail	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
v. Retaining wall/seat wall detail	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
vi. Bollard detail	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
g. Landscape			
i. Street tree detail	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
ii. Planting bed detail	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
iii. Shrub detail	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
iv. Ground cover detail	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
v. Fence Detail	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Transportation Analysis

Written/Graphic Information

	Required	Supplied	Received
1. Summary of the proposed development program for the lot or building	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
2. Identification of existing conditions:			
a. Site specific (including adjacent thoroughfares and building facades across the street from the site):			
i. Site access (pedestrian entrances to individual ground floor spaces and lobbies)	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
ii. Primary pedestrian routes	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
iii. Bicycling routes/lanes/paths, including all existing ground level bicycle parking	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
iv. Pavement markings including parking and loading areas, driveways, and circulation	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
v. On-street motor vehicle parking inventory, by type (metered, handicapped, no-parking, etc.)	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b. Within a quarter (1/4) mile study area:			
i. Public transportation including location, types, and service and ridership statistics for study area stations/stops	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
ii. Bicycling routes/lanes/paths	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
iii. Car and bicycle sharing locations	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
iv. Off-street motor vehicle parking inventory, by type (surface lot, above ground structure, underground structure)	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
3. Proposed future conditions:			
a. Site specific (including adjacent thoroughfares and building facades across the street from the site):			
i. Pedestrian, bicycle, and roadway infrastructure improvements	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
ii. Site access (pedestrian entrances to individual ground floor spaces and lobbies)	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
iii. Proposed curb cut or sidewalk changes, if applicable	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
iv. Short and long term bicycle parking/storage	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
v. Vehicular circulation, maneuvering, and access to the site/building(s)	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
vi. Off-street motor vehicle parking inventory, including layout, access, count, and size	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
vii. Car and bicycle sharing locations	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
viii. On-street motor vehicle parking inventory, by type (metered, handicapped, no-parking, etc.)	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
ix. Loading docks	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b. Within a quarter (1/4) mile study area			
i. Pedestrian, bicycle, and roadway infrastructure improvements	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
4. A multi-modal transportation demand analysis of existing and proposed future conditions following procedures outlined in the 2010 Highway Capacity Manual. Analysis shall include study of demand, capacity, trip distribution, and circulation for sidewalks, bike facilities, public transportation, travel lanes, and intersections for appropriate modes. AM, PM, and daily trip generation rates and modal splits shall be based on regional comparables of similar principal uses provided by the Office of Strategic Planning and Community Development or other sources deemed appropriate by the Planning Board. In the absence of applicable comparables, ITE Trip Generation (latest edition) data may be substituted.	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
5. A transportation demand management plan describing measures taken to encourage the reduction of single-occupancy vehicular trips including, but not limited to, the following:			
a. Ride-sharing incentives and information dissemination	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b. Set-asides for high-occupancy-vehicles, including number and location	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
c. Mass transit information dissemination	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
d. MBTA pass sales and subsidies	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
e. Direct station links or pedestrian connections	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
f. Encouragement of flexible work hours	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
g. Restrictions on service and good deliveries	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
6. A transportation demand monitoring plan	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Infrastructure Analysis

Written/Graphic Information

- | | Required | Supplied | Received |
|--------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|-------------------------------------|-------------------------------------|--------------------------|
| 1. Estimated water and electricity consumption, sewage generation, and energy requirements for the building(s) | <input checked="" type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| 2. Analysis of the capacity and adequacy of existing water, storm water drainage, and sewer, energy (including gas), and electrical communications (including telephone, fire alarm, computer, cable, etc) utility systems, and identification of required system upgrades or connections to accommodate the proposed development. Impacts due to any required system upgrades or connections requiring a significant public or utility investment, creating a significant disruption in vehicular or pedestrian circulation, or affecting any civic or recreation space or streetscape improvements shall be described. | <input checked="" type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |

Plan Drawings

- | | Required | Supplied | Received |
|----------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|-------------------------------------|-------------------------------------|--------------------------|
| 1. Twenty (1" to 20') scale or larger utility plan drawing identifying all existing and proposed utilities including but not limited to water, sanitary sewer, storm drainage, electrical, telephone, data, CATV, and natural gas utilities. | <input checked="" type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| 2. Construction drawings including, but not limited to, the following: | | | |
| a. Water/sewer | | | |
| i. Trench detail | <input checked="" type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| ii. Catch basin detail | <input checked="" type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| iii. Connection & tie-in detail | <input checked="" type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| iv. Manhole detail | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| v. Hydrant detail | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| vi. Detention basin and outlet detail | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| vii. Infiltration device detail | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |

Environmental Analysis

Written/Graphic Information

- | | Required | Supplied | Received |
|---------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|-------------------------------------|-------------------------------------|--------------------------|
| 1. Identification of existing M.G.L. Chapter 21E reports and/or other environmental assessments, analysis, clean-up studies, enforcement actions, or related documentation related to the site | <input checked="" type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| 2. Shadow analysis illustrating both existing and net new shadows according to the City of Somerville's Illustrating Building Shadows requirements | <input checked="" type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| 3. Wind analysis documenting potential pedestrian level winds (PLW's) adjacent to and in the vicinity of the project site according to the City of Somerville's Measuring Pedestrian Level Wind Impacts and identification of proposed mitigation for any uncomfortable winds generated at outdoor pedestrian areas | <input checked="" type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| 4. Solar glare analysis ensuring that no visual impairment or discomfort is caused on nearby thoroughfares, civic and recreation spaces, or pedestrian areas and that no solar heat buildup is caused in any nearby buildings due to reflective spot glare | <input checked="" type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| 5. Air quality analysis verifying that emissions from any parking facility and/or building heating and mechanical systems will not violate state or federal ambient air quality standards | <input checked="" type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| 6. Construction impact analysis and management program including, but not limited to, the following: | | | |
| a. A study of and mitigation plan for impacts on public safety from noise, dust, pollutant emissions, waste generation and disposal, and | <input checked="" type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| b. plans for construction vehicle access routes, on-site construction worker parking, designation of material storage methods and locations, and designation of construction hours | <input checked="" type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| c. A plan for solid and hazardous waste removal identifying any known toxic or hazardous wastes on or buried in the development site, pursuant to the requirements of M.G.L. Chapter 21E | <input checked="" type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| d. Analysis of sub-soil conditions, the potential for ground movement and settlement during construction, and the impact on adjacent buildings and utility lines and a description of foundation construction methodology | <input checked="" type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| e. Analysis of the impact of construction on groundwater levels and resulting effects on surrounding structures and building foundations | <input checked="" type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |



**COMMITMENT FOR TITLE INSURANCE
SCHEDULE A**

This commitment is subject to underwriting/rate approval if the proposed transaction exceeds
\$50,000,000.00

Case Number: C21292

Connection Number: 19263221

(Revised April 10, 2015)
(Revised April 13, 2015)
(Revised May 1, 2015)
(Revised June 29, 2015)
(Revised July 20, 2015)
(Revised July 23, 2015)
(Revised August 5, 2015)
(Revised August 7, 2015)
(Revised August 12, 2015)
(Revised August 18, 2015)

1. Effective Date: **August 18, 2015**

2. Policy or Policies to be issued:

(a) ALTA Owner's Policy - (6/17/06)

Amount: **\$291,040,268.46**

PROPOSED INSURED: **DW NP Property, LLC, a Delaware limited liability company**

(b) ALTA Loan Policy – (6/17/06)

Amount: **\$ NA**

PROPOSED INSURED: **NA**

3. Title to the fee simple estate or interest in the land described or referred to in this commitment is at the effective date hereof vested in:

CJUF III Northpoint LLC, by virtue of:

(a) Deed from Boston and Maine Corporation, dated as of August 19, 2010 and recorded in the Suffolk County Registry of Deeds in [Book 46807, Page 256](#) , and in the Middlesex South County Registry of Deeds in [Book 55212, Page 330](#) (PARCEL ONE)

(b) Deed from Boston and Maine Corporation dated June 11, 2014, and recorded in Middlesex South County Registry of Deeds in [Book 63899, Page 200](#) (PARCEL TWO)

(c) Quitclaim Deed from Boston and Maine Corporation, dated October 31, 2014 and recorded in the Suffolk County Registry of Deeds in Book 53735, Page 196, and recorded in the Middlesex South Registry of Deeds in Book 64544, Page 596. (PARCEL THREE)

4. The land referred to in this Commitment is described as follows:

All that certain parcel of land located in the cities of Cambridge, Somerville and Boston, Counties of Middlesex and Suffolk, Commonwealth of Massachusetts, all more particularly described in Exhibit "A" attached hereto and made a part hereof

NOTE: As hereinafter used, "recorded with Middlesex" shall mean "recorded with the Middlesex County Registry of Deeds (Southern District)" and "recorded with "Suffolk" shall mean "recorded with the Suffolk County Registry of Deeds".

Countersigned at Boston, Massachusetts

Schedule A – Part I

**Commonwealth Title Insurance Company
265 Franklin Street, 8th Floor
Boston, MA 02110-3113
(800) 621-0051**

Philip M. Saba
Vice President

NOTE: This Commitment is an offer to issue one or more title insurance policies. It is not a title insurance policy or an abstract of title, a report on the condition of title, legal opinion, opinion of title or similar representation of title. The Company shall have no liability other than that set forth in this Commitment. Any title search or examination that has been conducted for the preparation of this Commitment (i) has been conducted solely for the Company's determination of insurability under the terms of this Commitment and (ii) has not been conducted for any other person, including a Proposed Insured. Only a Proposed Insured, designated on Schedule A, may rely on this Commitment, and then only to acquire a title insurance policy from the Company in accordance with the terms and provisions of this Commitment. If the Company has not completed the Proposed Policy Amount on Schedule A, the offer of the Commitment is to issue a title insurance policy limited to an Amount of Insurance of no more than \$100,000.

EXHIBIT "A"

All those certain parcels of land located in the Cities of Cambridge and Somerville, Middlesex County and Boston, Suffolk County, Commonwealth of Massachusetts, and more particularly described as follows:

PARCEL ONE:

Parcel 1 on a plan entitled "North Point – "Central Park" Parcel Subdivision Plan of Land in Boston, Cambridge and Somerville, Massachusetts, Suffolk and Middlesex Counties", dated August 16, 2010, prepared by Gunther Engineering, a division of Digital Geographic Technologies, Inc., and recorded as [Plan 597 of 2010](#) (Middlesex) and [Plan Book 2010 Pages 270 and 271](#) (Suffolk).

Together with all right, title and interest in and to the private street known as Water Street as more particularly shown on the Plan, to the midline thereof and adjoining Parcel 1 described above, excepting therefrom so much of Water Street as may be owned in fee by the MBTA by virtue of the documents recorded with Middlesex at [Book 13117, Page 113](#) and at [Book 13156, Page 34](#).

Together with the right to use the "private" portion of Water Street as shown on the Plan.

Less and except the following described premises:

- (1) Parcel N set forth in a deed from CJUF III Northpoint LLC to NP Parcel N Owner LLC, dated December 12, 2012 and recorded with Middlesex in Book 60764, Page 223 and shown on Plan 937 of 2012; and
- (2) That certain parcel of land described in a deed from CJUF III Northpoint LLC to Massachusetts Bay Transportation Authority, dated as of November 20, 2013 and recorded with Middlesex in [Book 62977, Page 343](#)

PARCEL TWO (Central Park Parcel and Lot FP):

Parcel 1:

A certain parcel of land situated in the Commonwealth of Massachusetts, County of Middlesex, City of Cambridge, located on the northerly side of North Point Boulevard and being shown as "**CENTRAL PARK' PARCEL**" on a plan entitled "North Point – "Central Park Parcel", Subdivision Plan of Land in Boston, Cambridge and Somerville, Massachusetts, Suffolk and Middlesex Counties", dated August 16, 2010, prepared by Gunther Engineering, recorded in Middlesex County Registry of Deeds as [Plan 597 of 2010](#). Being more particularly bounded and described as follows:

Beginning at a point at the most southeasterly corner of the parcel, said point being on the northerly line of North Point Boulevard, thence running;

N 75° 22' 38" W	134.49 feet to a point, thence turning and running;
NORTHWESTERLY	by a curve to the right having a radius of 1979.00 feet a length of 48.78 feet to a point, thence turning and running;
N 73° 57' 53" W	632.12 feet to a point, thence turning and running;
NORTHWESTERLY	by a curve to the right having a radius of 224.00 feet and a length of 22.75 feet to a point, thence turning and running;
N 68° 08' 47" W	37.98 feet to a point, thence turning and running;
NORTHWESTERLY	by a curve to the right having a radius of 425.00 feet and a length of 94.54 feet to a point, said last six courses being by the northerly line of North Point Boulevard, thence turning and running;

N 57° 16' 47" W	256.98 feet to a point, thence turning and running;
N 11° 15' 41" E	91.06 feet to a point, thence turning and running;
S 81° 14' 36" E	200.13 feet to a point, thence turning and running;
S 73° 25' 09" E	14.60 feet to a point, thence turning and running;
S 81° 06' 44" E	67.29 feet to a point, thence turning and running;
S 74° 39' 04" E	33.61 feet to a point, thence turning and running;
S 81° 14' 36" E	178.52 feet to a point, thence turning and running;
S 85° 07' 47" E	40.27 feet to a point, thence turning and running;
NORTHEASTERLY	by a curve to the right having a radius of 319.00 feet and a length of 105.10 feet to a point, thence turning and running;
S 58° 22' 52" E	128.90 feet to a point, thence turning and running;
S 51° 45' 29" E	30.35 feet to a point, thence turning and running;
S 58° 22' 52" E	137.29 feet to a point, thence turning and running;
S 64° 25' 22" E	33.25 feet to a point, thence turning and running;
S 58° 22' 52" E	273.25 feet to a point, thence turning and running;
SOUTHEASTERLY	by a curve to the right having a radius of 30.08 feet and a length of 47.17 feet to a point, thence turning and running;
S 31° 28' 09" W	35.15 feet to a point, thence turning and running;
SOUTHWESTERLY	by a curve to the right having a radius of 4.00 feet and a length of 5.11 feet to the point of beginning. All of said courses being by Parcel 1.

Parcel 2:

A certain parcel of land situated in the City of Cambridge, County of Middlesex, Commonwealth of Massachusetts, being more particularly shown as "**Lot FP**" on a plan entitled "Condominium Site Plan of Sierra & Tango Condominium in Cambridge, Massachusetts, Middlesex County, Scale 1" = 30', 10 January 2008, Gunther Engineering", recorded with the Middlesex S. D. Registry of Deeds as [Plan No. 449 of 2008](#).

Together with the benefit of the following which are appurtenant to PARCEL ONE AND PARCEL TWO:

- a. Easements granted in Agreement of Covenants, Easements and Restrictions by and between CJUF III Northpoint LLC, Sierra Plus Tango LLC, as Trustee of Sierra + Tango Condominium Trust and Boston and Maine Corporation, dated April 1, 2011 and recorded with Middlesex in Book 56683, Page 375, and recorded with Suffolk in Book 47761, Page 232, as amended by First Amendment dated December 12, 2012 and recorded with Middlesex in Book 60764, Page 220, as affected by Assignment and Assumption of Northpoint Agreement of Covenants, Easements and Restrictions by and between CJUF III Northpoint LLC and DW NP Property, LLC dated as of August ___, 2015 and recorded with Middlesex in Book ___, Page ___ and with Suffolk in Book ___, Page ___.

PARCEL THREE:

A certain parcel of land situated partly in the City of Cambridge and partly in the City of Boston, Commonwealth of Massachusetts, Counties of Middlesex and Suffolk, located on Charlestown Avenue and

This Commitment is invalid unless the Insuring Provisions and Schedules A and B are attached.

being shown as **Parcel 2** on a plan entitled "North Point – "Central Park" Parcel, Subdivision Plan of Land in Boston, Cambridge and Somerville, Massachusetts, Suffolk and Middlesex Counties", dated August 16, 2010, prepared by Gunther Engineering, recorded in Middlesex County Registry of Deeds as [Plan 597 of 2010](#) and recorded in Suffolk County Registry of Deeds in [Plan Book 2010 Pages 270](#) and [271](#).

Together with the benefit of the following, as appurtenant to PARCEL ONE, PARCEL TWO and PARCEL THREE:

- A. Easements reserved by Grantor in paragraph 2 of that certain Release Deed and Grant of Easement from The Boston and Maine Corporation to Massachusetts Bay Transportation Authority, dated July 1, 1992, and recorded with the Suffolk County Registry of Deeds in [Book 17577, Page 179](#) and with Middlesex in [Book 22186, Page 479](#), as affected by Confirmatory Release Deed and Grant of Easement dated July 20, 1993 and recorded with Suffolk in [Book 19013, Page 1](#) and with Middlesex in [Book 24467, Page 562](#), which easement rights are in the property shown as "B&M Access Easement" on a plan recorded with the Suffolk County Registry of Deeds as Plan 17577, Page 179 and Middlesex County Southern District Registry of Deeds as Plan 546 of 1992.
- B. Easements to use and access the "Developer Crossover Easement Area", granted in the Northpoint Parking Easement Agreement between CJUF III, Northpoint LLC and Massachusetts Bay Transportation Authority, dated February 23, 2015, and recorded in Middlesex in Book 65048, Page 251.
- C. Easements set forth in that certain Easement Agreement by and between North Point Apartments Limited Partnership, Archstone North Point II LLC, AVB Maple Leaf Apartments Limited Partnership and CJUF III Northpoint LLC, dated July 31, 2015, and recorded in Suffolk in Book 54872, Page 23, and in Middlesex in Book 65856, Page 233.
- D. Easements set forth in Agreement of Covenants, Easements and Restrictions by and between CJUF III Northpoint LLC, NP Parcel N Owner LLC and the Boston and Maine Corporation, dated April 12, 2013 and recorded with Middlesex in Book 61625, Page 397 and recorded with Suffolk in Book 51306, Page 54, as affected by Northpoint Parcel N Deed Covenants by and between CJUF III Northpoint LLC and NP Parcel N Owner LLC dated April 11, 2013 and recorded with Middlesex in Book 61625, Page 496, affected by First Amendment to Northpoint Agreement of Covenants, Easements and Restrictions dated August 21, 2014, recorded with Suffolk in Book 53465 Page 37 and with Middlesex in Book 64210 Page 280, as affected by Second Amendment to Northpoint Agreement of Covenants, Easements and Restrictions dated June 12, 2015, recorded with Suffolk in Book 54628, Page 334, and recorded with Middlesex in Book 65567, Page 343, as affected by Assignment and Assumption of Northpoint Agreement of Covenants, Easements and Restrictions by and between CJUF III Northpoint LLC and DW NP Property, LLC dated as of August ____, 2015 and recorded with Middlesex in Book ____, Page ____ and with Suffolk in Book ____, Page ____.

NOTE: Acreage is not insured.



**COMMITMENT FOR TITLE INSURANCE
SCHEDULE B - SECTION 1**

REQUIREMENTS

Case Number: C21292

The following are the requirements to be complied with:

1. Payment to or for the account of the grantors or mortgagors of the full consideration for the estate or interest to be insured.
 2. Proper instrument(s) creating the estate or interest to be insured must be executed and duly filed for record to-wit:
 - (a) Duly authorized and executed deed from the party identified in Schedule A, Item 3, vesting fee title in the proposed insured owner; and
- Note: Section 6 of Notices of Activity and Use Limitations referenced in Schedule B, Section 2 below should be reviewed when drafting any conveyance documents.
3. Intentionally omitted.
 4. If Survey coverage is requested, prior to closing the company must be in receipt of a satisfactory survey and surveyor's report. The Survey exceptions set forth in Schedule B, Section 2, standard exception 2 will be eliminated or amended in accordance with the facts disclosed thereby.
 5. Receipt of properly executed Parties in Possession and Mechanic's Lien Affidavit in order to delete or modify exceptions set forth in Schedule B, Section 2, Standard Exceptions 1 and 3.
 6. If the insured premises have undergone construction or repair in the past 93 days, satisfactory evidence should be provided at or prior to closing that said improvements and/or repairs or alterations are complete and; that the contractor, subcontractors, labor and material men are all paid in full.
 7. In the event that the Company will be responsible for recording the closing documents,
 - (a) at least 24 hours before closing, provide this office with a copy of the form of deed, mortgage, and all other documents to be recorded; and
 - (b) along with the original documents for recording, provide this office with two copy sets of all documents to be recorded.
 8. Issuance of a final title insurance policy is conditioned upon payment of all title premium(s) due in connection with said policy(ies) at the present applicable rates as well as all examination and counsel fees and recording costs and charges incurred by the Company relative to this transaction.

(Continued)

**Schedule B – Section 1
Page 1**

**Schedule B – Section 1
Requirements Continued**

Case No: C21292

9. If the title to be vested in, or documents are to be executed by a **LIMITED PARTNERSHIP**, a current Certificate of limited partnership naming the General Partners ("Long Legal with General Partners") from the Secretary of State's of the Commonwealth of Massachusetts must be recorded/filed.
10. If title to be vested in or documents to be executed by a **LIMITED LIABILITY COMPANY**, provide a current Certificate of Good Standing for the LLC from the Massachusetts Secretary of State identifying:
 - (a) the managers of the limited liability company, if any;
 - (b) those persons who are authorized to act with respect to real estate instruments; and
 - (c) In the event that nobody is so authorized to execute real estate documents, obtain and record/file a certificate executed by the manager of the limited liability company stating that the persons executing the deed/mortgage in the name of the limited liability company are the incumbent members or managers and that such persons are empowered by the Operating Agreement to convey/mortgage the insured premises.
 - (d) There may be circumstances in which an LLC does not have managers and the identity of persons authorized to execute real estate documents is not disclosed in the Secretary of State filing. In those circumstances, the statute provides that a good faith third party purchaser may rely upon a certificate executed by a person identified at the Secretary of State's Office as a manager or as a person authorized to execute documents to be filed with the Secretary of State certifying as to (i) the incumbency of any manager or member and (ii) the authority of any persons to act on behalf of the LLC whether or not such person is identified in the Secretary of State filing. M.G.L. c. 156C, s. 67. Such a certificate shall be binding upon the LLC in favor of a person relying in good faith thereon notwithstanding inconsistent provisions in the operating agreement. M.G.L. c. 156C, s. 67. The protection afforded by the statute applies equally to domestic LLC's and foreign LLC's, which have registered with the Secretary of State.
11. With respect to **CJUF III Northpoint LLC (Seller)**, provide a current original Certificate of Good Standing for the LLC from the Massachusetts Secretary of State identifying:
 - (a) the managers of the limited liability company, if any;
 - (b) those persons who are authorized to act with respect to real estate instruments; and
 - (c) In the event that nobody is so authorized to execute real estate documents, obtain and record/file a certificate executed by the manager of the limited liability company stating that the persons executing the deed/mortgage in the name of the limited liability company are the incumbent members or managers and that such persons are empowered by the Operating Agreement to convey/mortgage the insured premises.
 - (d) There may be circumstances in which an LLC does not have managers and the identity of persons authorized to execute real estate documents is not disclosed in the Secretary of State filing. In those circumstances, the statute provides that a good faith third party purchaser may rely upon a certificate executed by a person identified at the Secretary of State's Office as a manager or as a person authorized to execute documents to be filed with the Secretary of State certifying as to (i) the incumbency of any manager or member and (ii) the authority of any persons to act on behalf of the LLC whether or not such person is identified in the Secretary of State filing. M.G.L. c. 156C, s. 67. Such a certificate shall be binding upon the LLC in favor of a person relying in good faith thereon notwithstanding inconsistent provisions in the operating agreement. M.G.L. c. 156C, s. 67. The protection afforded by the statute applies equally to domestic LLC's and foreign LLC's, which have registered with the Secretary of State.

Note: In the event the Seller is classified for the taxable year as a corporation for federal income tax purposes, the Company must be in receipt of a Commonwealth of Massachusetts, Department of Revenue, Waivers of Excise Tax Lien if the parcel described in Schedule A constitutes all or substantially all of the assets of the seller. If

the seller will NOT be conveying all or substantially all of their assets in Massachusetts, the Deed should contain statement to that effect. If the seller is not classified for the taxable year as a corporation for federal income tax purposes, then the Deed should contain a statement to that effect. (Mass. St. 2008, c. 173, s. 27, amending M.G.L. c. 62C, s. 51).

12. Upon full disclosure to the Company of the nature and scope of this transaction and our review and approval of the closing documents, including updated certifications of title, the Company reserves the right to raise such other and further exceptions and requirements as it deems appropriate.
13. Duly authorized and executed Discharge/Termination of the following items: **NONE**



**COMMITMENT FOR TITLE INSURANCE
SCHEDULE B - SECTION 2**

EXCEPTIONS FROM COVERAGE

Case Number: C21292

The policy or policies to be issued will contain exceptions to the following unless the same are disposed of to the satisfaction of the Company:

1. Rights of the following parties as tenants or licensees only, with no right or option to purchase any portion of the land (schedule of tenants to be provided to Company).
2. Intentionally omitted.
3. Intentionally omitted.
4. Intentionally omitted.
5. Liens for taxes and municipal charges which become due and payable subsequent to the date of said policy.
6. Right of others legally entitled thereto to use the "private" portion of Water Street as shown on the Plan referred to in Exhibit A above (affects the portion of PARCEL ONE within Water Street as shown on the ALTA Survey (defined below).
7. Electric Utility Easement from Ogden Realty Limited Partnership to Cambridge Electric Light Company dated June 22, 1989 and recorded with Middlesex in [Book 19905, Page 539](#). (Affects PARCEL ONE and Parcel 1 of PARCEL TWO only)

This Policy insures that this Easement is located as shown on the ALTA Survey and affects only East Street and a portion of the property adjacent thereto, as shown on the Exhibit A sketch attached to the easement set forth above.

8. Report and Findings of the Superior Court Commission on the Abolition of Grade Crossing recorded August 4, 1900 with Suffolk in [Book 2700, Page 243](#), affecting the location of Austin Street and the draw bridge over the Millers River at the Prison Point Bridge and Report and Findings of the Special Commission on the Alterations of the Grade Crossing of Prison Point Street in Cambridge dated March 29, 1899 and recorded May 2, 1900 with Middlesex in [Book 2815, Page 145](#), establishing the layout of Prison Point Street in Cambridge to Austin Street in Boston over a viaduct 50 feet wide.

This Policy insures that the documents referred to in this Item affect only the land located under the "Prison Point Bridge" a/k/a Gilmore Bridge as shown on the ALTA Survey.

9. Prison Point Bridge Taking dated September 25, 1972 by the Commonwealth of Massachusetts, Metropolitan District Commission, and recorded with Suffolk in Book 8540, Page 651 and recorded with Middlesex in [Book 12222, Page 250](#).

This Policy insures that this Taking affects only the land located under the "Prison Point Bridge" a/k/a Gilmore Bridge and a strip of land adjacent thereto, as shown on the ALTA Survey.

10. Agreement regarding drainage set forth in Paragraph 6 of the Release Deed and Grant of Easement between the Boston and Maine Corporation and the Massachusetts Bay Transportation Authority dated July 1, 1992 and recorded with Suffolk in [Book 17577, Page 179](#) and with Middlesex in [Book 22186, Page 479](#), as affected by Confirmatory Release Deed and Grant of Easement dated July 20, 1993 and recorded with Suffolk in [Book 19013, Page 1](#) and with Middlesex in [Book 24467, Page 562](#)
11. Easements for access set forth in Deed from the Boston and Maine Railroad to Keith Fulton & Sons, Inc. dated September 14, 1961 and recorded with Middlesex in [Book 9891, Page 128](#), as affected by the City of Cambridge Taking for public parking and transportation dated November 10, 1980 and recorded with Middlesex in [Book 14151, Page 141](#).

This Policy insures that the rights and easements referred to in this Item affect East Street and the 40' wide Access Drive as shown on the ALTA Survey. This policy further insures that the rights to use East Street do not extend into any portion of the Central Park Parcel.

12. Easement set forth in Deed from the Boston and Maine Railroad to Leo Pistorino dated December 24, 1964 and recorded with Middlesex in [Book 10721, Page 201](#), conveying the area shown on the plan recorded with Middlesex in [Book 10699, Page End](#), as is affected by rights granted by Leo M. Pistorino, et al, Trustees to P.W. Rounsevell, Inc. by deed dated August 22, 1966 and recorded with Middlesex in [Book 11193, Page 436](#), as further affected by rights granted by Leo M. Pistorino et al, Trustees to Pappas industrial Properties, Inc. by deed dated October 13, 1977 and recorded with Middlesex in [Book 13309, Page 047](#), as further affected by rights granted by Leo M. Pistorino, et al, Trustees to Bornstein & Fisher, Inc. by deed dated January 16, 1978 and recorded with Middlesex in [Book 13375, Page 339](#).

This Policy insures that the rights and easements in No. 12 above affect only that portion of East Street shown on the ALTA Survey.

13. Taking of Utility Easement by the Commonwealth of Massachusetts by its Division of Capital Planning and Operations dated March 27, 1987 and recorded with Middlesex in [Book 17987, Page 054](#), as affected by Assignment dated March 27, 1987 to Cambridge Electric Light Company recorded with Middlesex in [Book 17987, Page 057](#), which utility easements are located as shown on the ALTA Survey.
14. Terms and provisions of M.G.L. Chapter 40, Section 54A.
15. Ownership of Prison Point (Gilmore) Bridge by the Commonwealth of Massachusetts, which is located as shown on the ALTA Survey.
16. Easement for utility purposes from Boston and Maine Corporation et al to Verizon New England Inc. dated December 29, 2006 and recorded with Middlesex in [Book 49883, Page 423](#), as affected by Consent and Joinder recorded with Middlesex in [Book 49883, Page 433](#), and as further affected by First Amendment to Easement, dated July 17, 2015, and recorded in Suffolk in Book 54808, Page 257, and in Middlesex in Book 65780, Page 442, located as shown on the ALTA Survey.
17. Easement for utility purposes from Boston and Maine Corporation et al to NStar Gas Company dated December 29, 2006 and recorded with Middlesex in [Book 49883, Page 437](#), as affected by Consent and Joinder recorded in [Book 49883, Page 445](#), located as shown on the ALTA Survey and affects only the private ways shown on the ALTA Survey.
18. Easement for utility purposes from Boston and Maine Corporation et al to Comcast of Massachusetts I, LLC dated December 29, 2006 and recorded with Middlesex in [Book 49883, Page 447](#), as affected by Consent and Joinder recorded with Middlesex in [Book 49883, Page 454](#), located as shown on the ALTA Survey and affects only the private ways as shown on the ALTA Survey.
19. Easement for utility purposes from Boston and Maine Corporation et al to Cambridge Electric Light Company dated December 29, 2006 and recorded with Middlesex in [Book 49883, Page 474](#), as affected by Consent and Joinder recorded in [Book 49883, Page 485](#), located as shown on the ALTA Survey and affects only the private ways as shown on the ALTA Survey.as shown on the Survey.

This Commitment is invalid unless the Insuring Provisions and Schedules A and B are attached.

20. Intentionally omitted.
21. Rights of Clear Channel Outdoor Inc., pursuant to a license dated May 1, 2012 relative to the Billboard located on PARCEL ONE, as shown on the ALTA Survey, and any and all claims arising thereunder.
22. Intentionally omitted.
23. Intentionally omitted.
24. Intentionally omitted.
25. Easements, covenants, conditions and restrictions, including but not limited to common area assessments and charges set forth in Agreement of Covenants, Easements and Restrictions by and between CJUF III Northpoint LLC, Sierra Plus Tango LLC, as Trustee of Sierra + Tango Condominium Trust, and Boston and Maine Corporation, dated April 1, 2011 and recorded in [Book 56683, Page 375](#) (Middlesex) and Book 47761, Page 232 (Suffolk), as affected by First Amendment dated December 12, 2012 and recorded with Middlesex in [Book 60764, Page 220](#).
26. Easements, covenants, conditions and restrictions, including but not limited to common area assessments and charges set forth in Agreement of Covenants, Easements and Restrictions by and between CJUF III Northpoint LLC, NP Parcel N Owner LLC and the Boston and Maine Corporation, dated April 12, 2013 and recorded with Middlesex in [Book 61625, Page 397](#) and recorded with Suffolk in [Book 51306, Page 54](#), as affected by Northpoint Parcel N Deed Covenants by and between CJUF III Northpoint LLC and NP Parcel N Owner LLC dated April 11, 2013 and recorded with Middlesex in [Book 61625, Page 496](#), affected by First Amendment To Northpoint Agreement of Covenants, Easements and Restrictions recorded In Suffolk Book 53465 Page 37 and in Middlesex in Book 64210 Page 280, as affected by Second Amendment to Northpoint Agreement of Covenants, Easements and Restrictions, recorded with Suffolk in Book 54628, Page 334, and recorded in Middlesex in Book 65567, Page 343.
27. Notice of Activity and Use Limitation by Boston and Maine Corporation dated June 13, 2013 and recorded in [Book 62072, Page 281](#) (affects Parcel 2 of PARCEL TWO), as shown on the ALTA Survey.
28. Notice of Activity and Use Limitation by Boston and Maine Corporation dated June 13, 2013 and recorded with Middlesex in [Book 62072, Page 314](#) (affects Parcel 1 of PARCEL TWO), as shown on the ALTA Survey.
29. Rights of others to use a private way delineated as the "50' Wide Access Drive (formerly East Street) on the Subdivision Plan of Land recorded as [Plan 597 of 2010](#)) for all purposes for which streets or ways are used in the City of Cambridge, however, said rights of others do not extend to any portion of the land located within the bounds of Central Park Parcel, as shown on the ALTA Survey.
30. Easement for Electric Service by and between NP Parcel Owner LLC, CJUF III Northpoint LLC and Boston and Maine Corporation to NStar Electric Company, dated December 11, 2013 and recorded in [Book 63310, Page 347](#) (affects Parcel One), which easement is located as shown on the ALTA Survey, as affected by First Amended to Easement for Electric Service, dated June 16, 2015, and recorded in Middlesex in Book 65660, Page 84.
31. Sewer and Drainage Taking for sewer and drainage easements dated July 10, 1975 by the Commonwealth of Massachusetts, Metropolitan District Commission, and recorded with Suffolk in [Book 8801, Page 515](#) and recorded with Middlesex in Book 12828, Page 392, which easements are located as shown on the ALTA Survey (Affects Parcel Three only).
32. Intentionally omitted.

33. Development Regulatory Agreement between Boston Redevelopment Authority and CJUF III Northpoint LLC dated July 29, 2014 and recorded with Suffolk in [Book 53319, Page 90 \(Affects Parcel One and Parcel Three only\)](#).
34. Terms and provisions of a Lease by and CJUF III Northpoint LLC (Landlord) and Sierra Plus Tango LLC (Tenant) dated April 1, 2012, a Notice of which is recorded with Middlesex in [Book 60222, Page 132](#).
35. Intentionally omitted.
36. Planning Board Decision issued by the City of Somerville October 16, 2014, and recorded in Middlesex in Book 64661, Page 25.
37. Notice of Activity and Use Limitation recorded with Middlesex in [Book 62072, Page 351](#). (Affects a portion of PARCEL ONE as shown on the ALTA Survey)
38. Intentionally omitted.

NOTE: Although specifically excluded from the coverage of this policy, the following matters a and b, which are recorded with the Middlesex South District Registry of Deeds, are provided for informational purposes only:

- a. Order of Conditions dated June 29, 1992 issued by the Somerville Conservation Commission in favor of Massachusetts Bay Transportation Authority and Boston and Maine Railroad and recorded November 4, 1992 in Book 22580, Page 215.
- b. MA DEP Permit for Sewer System Extension recorded with Middlesex in [Book 58497, Page 1](#).
39. Intentionally omitted.
40. Intentionally omitted.
41. Intentionally omitted.
42. Intentionally omitted.
43. Intentionally omitted.
44. Intentionally omitted.
45. Intentionally omitted.
46. Zoning Decision, City of Cambridge Planning Board Case No. 179, Notice of which is recorded with Middlesex in [Book 44824, Page 370](#), as affected by Minor Amendment No. 1, recorded in [Book 62918, Page 289](#), as further affected by Minor Amendment No. 2, recorded in [Book 62918, Page 306](#), as further affected by Amendment No. 3 (Major), recorded in [Book 62918, Page 308](#), as affected by City of Cambridge Planning Board Notice of Decision, dated February 13, 2015 and recorded in the Suffolk County Registry of Deeds in Book 54166, Page 121, and in the Middlesex South County Registry of Deeds in Book 65040, Page 490, as affected by Decision by the City of Cambridge Planning Board, dated recorded in [Book 65040, Page 490](#).
47. Intentionally omitted.
48. Intentionally omitted.
49. Survey entitled "ALTA/ACSM Land Title Survey, Northpoint in Cambridge and Somerville, MA (Middlesex County), and Boston, MA (Suffolk County), dated April 10, 2015, last revised August --, 2015, prepared by Beals and Thomas, Inc, (the "ALTA Survey") discloses the following matters:

- a. There is an underground oil pipe crossing the property line;

This Commitment is invalid unless the Insuring Provisions and Schedules A and B are attached.

- b. There are various drain lines crossing the property line;
- c. There are overhead wires crossing the property line;
- d. There is a guardrail crossing the property line;
- e. There is a fence encroaching onto land now or formerly of MBTA;
- f. There are concrete blocks encroaching onto land now or formerly of MBTA;
- g. There are various water lines crossing the property line;
- h. Intentionally omitted;
- i. There is a fence and gate crossing the property line;
- j. Intentionally omitted;
- k. There are gravel roadways crossing the property line;
- l. The BIT. Conc. Drive is located outside of the access easement;
- m. There are three (3) 48" steel pipes that enter the premises;
- n. Intentionally omitted;
- o. There is a concrete footing below grade that extends over the property line approximately 0.7 feet;
- p. Intentionally omitted;
- q. There is a jersey barrier wall encroaching onto land now or formerly of AVB Maple Leaf Apartments L.P. by approx. 0.2';
- r. There is a utility pole located on land now or formerly of MBTA and overhead wires crossing the property line;
- s. There is an edge of pavement and parking stripes encroaching onto the premises from land now or formerly of MBTA;
- t. There is a sewer line crossing onto land now or formerly of MBTA;
- u. There is an electric line crossing onto land now or formerly of MBTA;
- v. There is access and parking stripes crossing the property line between land now or formerly of MBTA and CJUF III Northpoint LLC;
- w. Intentionally omitted;
- x. Intentionally omitted;
- y. There is a wood and metal pile straddling the property line between land now or formerly CJUF III Northpoint LLC and MBTA;
- z. There is debris located on the premises and crossing the property line between land now or formerly of CJUF III Northpoint LLC and MBTA;
- aa. Intentionally omitted;

- bb. The concrete wall with cap extends over the property line by 0.02’;
 - cc. The concrete wall with cap extends over the property line by 0.10’; and
 - dd. The building façade extends over the property line by 0.07’.
- 50. Intentionally omitted.
 - 51. Northpoint Parking Easement Agreement, dated February 23, 2015, by and between CJUF III Northpoint LLC and the Massachusetts Bay Transportation Authority, recorded with the Middlesex South County Registry of Deeds in Book 65048, Page 251.
 - 52. Easement Agreement (Drainage Infrastructure) by and between CJUF III Northpoint LLC and the Massachusetts Bay Transportation Authority, dated May 12, 2015, and recorded in Middlesex in Book 65383 Page 1 and recorded in Suffolk in Book 54469, Page 275
 - 53. [Notice](#) of Activity and Use Limitation, dated July 20, 2015, and recorded in Middlesex in Book 65752, Page 408.
 - 54. Easement Agreement by and between North Point Apartments Limited Partnership, Archstone North Point II LLC, AVB Maple Leaf Apartments Limited Partnership and CJUF III Northpoint LLC, dated July 31, 2015 and recorded in Suffolk in Book 54872, Page 23, and in Middlesex in Book 65856, Page 233.

NOTE: If policy is to be issued in support of a mortgage loan, attention is directed to the fact that the Company can assume no liability under its policy, the closing instructions, or Insured Closing Service for compliance with the requirements of any consumer credit protection or truth in lending law in connection with said mortgage loan.

NOTE: This commitment omits any covenant, condition or restriction referred to above which is based on race, color, religion, sex, handicap, familial status or national origin, unless and only to the extent that the restriction is not in violation of state or federal law, or relates to a handicap, but does not discriminate against handicapped people.

Fidelity National Financial, Inc.
Privacy Statement

Fidelity National Financial, Inc. and its subsidiaries ("FNF") respect the privacy and security of your non-public personal information ("Personal Information") and protecting your Personal Information is one of our top priorities. This Privacy Statement explains FNF's privacy practices, including how we use the Personal Information we receive from you and from other specified sources, and to whom it may be disclosed. FNF follows the privacy practices described in this Privacy Statement and, depending on the business performed, FNF companies may share information as described herein.

Personal Information Collected

We may collect Personal Information about you from the following sources:

- Information we receive from you on applications or other forms, such as your name, address, social security number, tax identification number, asset information, and income information;
- Information we receive from you through our Internet websites, such as your name, address, email address, Internet Protocol address, the website links you used to get to our websites, and your activity while using or reviewing our websites;
- Information about your transactions with or services performed by us, our affiliates, or others, such as information concerning your policy, premiums, payment history, information about your home or other real property, information from lenders and other third parties involved in such transaction, account balances, and credit card information; and
- Information we receive from consumer or other reporting agencies and publicly recorded documents.

Disclosure of Personal Information

We may provide your Personal Information (excluding information we receive from consumer or other credit reporting agencies) to various individuals and companies, as permitted by law, without obtaining your prior authorization. Such laws do not allow consumers to restrict these disclosures. Disclosures may include, without limitation, the following:

- To insurance agents, brokers, representatives, support organizations, or others to provide you with services you have requested, and to enable us to detect or prevent criminal activity, fraud, material misrepresentation, or nondisclosure in connection with an insurance transaction;
- To third-party contractors or service providers for the purpose of determining your eligibility for an insurance benefit or payment and/or providing you with services you have requested;
- To an insurance regulatory authority, or a law enforcement or other governmental authority, in a civil action, in connection with a subpoena or a governmental investigation;
- To companies that perform marketing services on our behalf or to other financial institutions with which we have joint marketing agreements and/or
- To lenders, lien holders, judgment creditors, or other parties claiming an encumbrance or an interest in title whose claim or interest must be determined, settled, paid or released prior to a title or escrow closing.

We may also disclose your Personal Information to others when we believe, in good faith, that such disclosure is reasonably necessary to comply with the law or to protect the safety of our customers, employees, or property and/or to comply with a judicial proceeding, court order or legal process.

Effective Date: 5/1/2008

Disclosure to Affiliated Companies – We are permitted by law to share your name, address and facts about your transaction with other FNF companies, such as insurance companies, agents, and other real estate service providers to provide you with services you have requested, for marketing or product development research, or to market products or services to you. We do not, however, disclose information we collect from consumer or credit reporting agencies with our affiliates or others without your consent, in conformity with applicable law, unless such disclosure is otherwise permitted by law.

Disclosure to Nonaffiliated Third Parties – We do not disclose Personal Information about our customers or former customers to nonaffiliated third parties, except as outlined herein or as otherwise permitted by law.

Confidentiality and Security of Personal Information

We restrict access to Personal Information about you to those employees who need to know that information to provide products or services to you. We maintain physical, electronic, and procedural safeguards that comply with federal regulations to guard Personal Information.

Access to Personal Information/

Requests for Correction, Amendment, or Deletion of Personal Information

As required by applicable law, we will afford you the right to access your Personal Information, under certain circumstances to find out to whom your Personal Information has been disclosed, and request correction or deletion of your Personal Information. However, FNF's current policy is to maintain customers' Personal Information for no less than your state's required record retention requirements for the purpose of handling future coverage claims.

For your protection, all requests made under this section must be in writing and must include your notarized signature to establish your identity. Where permitted by law, we may charge a reasonable fee to cover the costs incurred in responding to such requests. Please send requests to:

Chief Privacy Officer
Fidelity National Financial, Inc.
601 Riverside Avenue
Jacksonville, FL 32204

Changes to this Privacy Statement

This Privacy Statement may be amended from time to time consistent with applicable privacy laws. When we amend this Privacy Statement, we will post a notice of such changes on our website. The effective date of this Privacy Statement, as stated above, indicates the last time this Privacy Statement was revised or materially changed.

COMMITMENT FOR TITLE INSURANCE

Issued by **Commonwealth Land Title Insurance Company**



COMMONWEALTH LAND TITLE INSURANCE COMPANY, a Nebraska corporation ("Company"), for a valuable consideration, commits to issue its policy or policies of title insurance, as identified in Schedule A, in favor of the Proposed Insured named in Schedule A, as owner or mortgagee of the estate or interest in the land described or referred to in Schedule A, upon payment of the premiums and charges and compliance with the Requirements; all subject to the provisions of Schedules A and B and to the Conditions of this Commitment.


This Commitment shall be effective only when the identity of the Proposed Insured and the amount of the policy or policies committed for have been inserted in Schedule A by the Company.

All liability and obligation under this Commitment shall cease and terminate Six (6) months after the Effective Date or when the policy or policies committed for shall issue, whichever first occurs, provided that the failure to issue the policy or policies is not the fault of the Company.

The Company will provide a sample of the policy form upon request.

IN WITNESS WHEREOF, the Company has caused this Commitment to be signed with the facsimile signatures of its President and Secretary and sealed as required by its By-Laws.

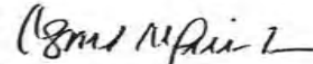
Attest:


Secretary



COMMONWEALTH LAND TITLE INSURANCE COMPANY

By:


President

CONDITIONS

1. The term mortgage, when used herein, shall include deed of trust, trust deed, or other security instrument.
2. If the proposed Insured has or acquired actual knowledge of any defect, lien, encumbrance, adverse claim or other matter affecting the estate or interest or mortgage thereon covered by this Commitment other than those shown in Schedule B hereof, and shall fail to disclose such knowledge to the Company in writing, the Company shall be relieved from liability for any loss or damage resulting from any act of reliance hereon to the extent the Company is prejudiced by failure to so disclose such knowledge. If the proposed Insured shall disclose such knowledge to the Company, or if the Company otherwise acquires actual knowledge of any such defect, lien, encumbrance, adverse claim or other matter, the Company at its option may amend Schedule B of this Commitment accordingly, but such amendment shall not relieve the Company from liability previously incurred pursuant to paragraph 3 of these Conditions.
3. Liability of the Company under this Commitment shall be only to the named proposed Insured and such parties included under the definition of Insured in the form of policy or policies committed for and only for actual loss incurred in reliance hereon in undertaking in good faith (a) to comply with the requirements hereof, or (b) to eliminate exceptions shown in Schedule B, or (c) to acquire or create the estate or interest or mortgage thereon covered by this Commitment. In no event shall such liability exceed the amount stated in Schedule A for the policy or policies committed for and such liability is subject to the insuring provisions and Conditions and the Exclusions from Coverage of the form of policy or policies committed for in favor of the proposed Insured which are hereby incorporated by reference and are made a part of this Commitment except as expressly modified herein.
4. This Commitment is a contract to issue one or more title insurance policies and is not an abstract of title or a report of the condition of title. Any action or actions or rights of action that the proposed Insured may have or may bring against the Company arising out of the status of the title to the estate or interest or the status of the mortgage thereon covered by this Commitment must be based on and are subject to the provisions of this Commitment.

COMMITMENT FOR TITLE INSURANCE

American Land Title Association (2006)

Issued By

**Commonwealth Land Title
Insurance Company**

Commonwealth Land Title Insurance Company



P.O. Box 45023
Jacksonville, Florida 32232-5023

Form B 1004-319



Bk: 65949 Pg: 156 Doc: DEED
Page: 1 of 9 08/21/2015 10:59 AM

DEED

CJUF III NORTHPOINT LLC, Delaware limited liability company, with an address of 2000 Avenue of the Stars, 11th Floor, Los Angeles, California 90067 ("Grantor"), for consideration paid and full consideration of Two Hundred Ninety One Million Forty Thousand Two Hundred Sixty Eight and 46/100 Dollars (\$291,040,268.46), and other good and adequate consideration, the receipt and adequacy of which are acknowledged, hereby grants **DW NP PROPERTY, LLC**, a Delaware limited liability company, with an address c/o DivcoWest Real Estate Services, LLC, 575 Market Street, 35th Floor, San Francisco, CA 94105 ("Grantee"), with **QUITCLAIM COVENANTS**, the land, together with any improvements thereon, located in the Cities of Cambridge and Somerville, Middlesex County, Massachusetts, and the City of Boston, Suffolk County, Massachusetts, as more particularly described in EXHIBIT A attached hereto and made a part hereof (the "Real Property").

The conveyance is made together with and subject to all recorded easements, conditions, restrictions and agreements and all other matters of record that lawfully apply to the property hereby conveyed.

The Real Property is subject to Notices of Activity and Use Limitation dated June 13, 2013 and recorded with the Middlesex County, Southern District Registry of Deeds (the "Middlesex Registry") in Book 62072, Page 351, in Book 62072, Page 281, and in Book 62072, Page 314, and the Notice of Activity and Use Limitation dated July 20, 2015 and recorded with the Middlesex Registry in Book 65752, Page 408.

Grantor has not elected to be treated as a corporation for Federal Income Tax purposes.

For Grantor's title, see (i) Deed of Boston and Maine Corporation dated as of August 19, 2010, recorded in the Suffolk County Registry of Deeds (the "Suffolk Registry") at Book 46807, Page 256 and in the Middlesex Registry in Book 55212, Page 330, (ii) Deed of Boston and Maine Corporation, dated June 11, 2014 and recorded in the Middlesex Registry in Book 63899, Page 200, and (iii) Deed of Boston and Maine Corporation dated October 31, 2014, recorded in the Suffolk Registry in Book 53735, Page 196, and recorded in the Middlesex Registry in Book 64544, Page 596.

Property: Land in Cambridge, Somerville and Boston, Massachusetts located on Charlestown Avenue, North Point Boulevard, East Street and Water Street, and Dawes St

This Deed is for the conveyance of the Real Property, which is located in both Middlesex County and Suffolk County, Massachusetts. Deed Excise Stamps based upon the total consideration in the amount of ~~\$291,000,000~~ have been affixed to the duplicate original of this deed recorded this day in Suffolk County. 291,040,268.46

[Balance of page intentionally left blank]

Witness our hand and seal as of the 14th day of August, 2015.

CJUF III NORTHPOINT LLC,
a Delaware limited liability company

By: _____

Name:

Jonathan M. Kaplan

Title:

Authorized Signatory

Legal	Acq/AM
<u>12</u>	<u>WKS</u>

A notary public or other officer completing this certificate verifies only the identity of the individual who signed the document to which this certificate is attached, and not the truthfulness, accuracy, or validity of that document.

ACKNOWLEDGEMENT

STATE OF CALIFORNIA
COUNTY OF Los Angeles

ss:

On 8/14/2015 before me, Rachel E Benitez, a Notary Public,

(insert name and title of the officer)

personally appeared Jonathan Kaplan

who proved to me on the basis of satisfactory evidence to be the person(s) whose name(s) is are subscribed to the within instrument and acknowledged to me that he she/they executed the same in his her/their authorized capacity(ies), and that by his her/their signature(s) on the instrument the person(s), or the entity upon behalf of which the person(s) acted, executed the instrument.

I certify under PENALTY OF PERJURY under the laws of the State of California that the foregoing paragraph is true and correct.

WITNESS my hand and official seal.

Signature

Rachel E. Benitez

(Seal)



EXHIBIT A

PARCEL ONE:

Parcel 1 on a plan entitled "North Point – 'Central Park' Parcel Subdivision Plan of Land in Boston, Cambridge and Somerville, Massachusetts, Suffolk and Middlesex Counties", dated August 16, 2010, prepared by Gunther Engineering, a division of Digital Geographic Technologies, Inc., and recorded as Plan 597 of 2010 (Middlesex) and Plan Book 2010 Pages 270 and 271 (Suffolk).

Together with all right, title and interest in and to the private street known as Water Street as more particularly shown on the Plan, to the midline thereof and adjoining Parcel 1 described above, excepting therefrom so much of Water Street as may be owned in fee by the MBTA by virtue of the documents recorded in Middlesex South District Registry of Deeds at Book 13117, Page 113 and at Book 13156, Page 34.

Together with the right to use the "private" portion of Water Street as shown on the Plan.

Less and except the following described premises:

- (1) Parcel N set forth in a deed from CJUF III Northpoint LLC to NP Parcel N Owner LLC, dated December 12, 2012 and recorded with Middlesex South District Registry of Deeds in Book 60764, Page 223 and shown on Plan 937 of 2012; and
- (2) That certain parcel of land described in a deed from CJUF III Northpoint LLC to Massachusetts Bay Transportation Authority, dated as of November 20, 2013 and recorded with Middlesex South District Registry of Deeds in Book 62977, Page 343.

PARCEL TWO (Central Park Parcel and Lot FP):**Parcel 1:**

A certain parcel of land situated in the Commonwealth of Massachusetts, County of Middlesex, City of Cambridge, located on the northerly side of North Point Boulevard and being shown as "CENTRAL PARK PARCEL" on a plan entitled "North Point – 'Central Park Parcel', Subdivision Plan of Land in Boston, Cambridge and Somerville, Massachusetts, Suffolk and Middlesex Counties" dated August 16, 2010, prepared by Gunther Engineering, recorded in Middlesex County Registry of Deeds as Plan 597 of 2010. Being more particularly bounded and described as follows:

Beginning at a point at the most southeasterly corner of the parcel, said point being on the northerly line of North Point Boulevard, thence running;

N 75° 22' 38" W 134.49 feet to a point, thence turning and running;

NORTHWESTERLY by a curve to the right having a radius of 1979.00 feet a length of

	48.78 feet to a point, thence turning and running;
N 73° 57' 53" W	632.12 feet to a point, thence turning and running;
NORTHWESTERLY	by a curve to the right having a radius of 224.00 feet and a length of 22.75 feet to a point, thence turning and running;
N 68° 08' 47" W	37.98 feet to a point, thence turning and running;
NORTHWESTERLY	by a curve to the right having a radius of 425.00 feet and a length of 94.54 feet to a point, said last six courses being by the northerly line of North Point Boulevard, thence turning and running;
N 57° 16' 47" W	256.98 feet to a point, thence turning and running;
N 11° 15' 41" E	91.06 feet to a point, thence turning and running;
S 81° 14' 36" E	200.13 feet to a point, thence turning and running;
S 73° 25' 09" E	14.60 feet to a point, thence turning and running;
S 81° 06' 44" E	67.29 feet to a point, thence turning and running;
S 74° 39' 04" E	33.61 feet to a point, thence turning and running;
S 81° 14' 36" E	178.52 feet to a point, thence turning and running;
S 85° 07' 47" E	40.27 feet to a point, thence turning and running;
NORTHEASTERLY	by a curve to the right having a radius of 319.00 feet and a length of 105.10 feet to a point, thence turning and running;
S 58° 22' 52" E	128.90 feet to a point, thence turning and running;
S 51° 45' 29" E	30.35 feet to a point, thence turning and running;
S 58° 22' 52" E	137.29 feet to a point, thence turning and running;
S 64° 25' 22" E	33.25 feet to a point, thence turning and running;
S 58° 22' 52" E	273.25 feet to a point, thence turning and running;
SOUTHEASTERLY	by a curve to the right having a radius of 30.08 feet and a length of 47.17 feet to a point, thence turning and running;
S 31° 28' 09" W	35.15 feet to a point, thence turning and running;

SOUTHWESTERLY by a curve to the right having a radius of 4.00 feet and a length of 5.11 feet to the point of beginning. All of said courses being by Parcel 1.

Parcel 2:

A certain parcel of land situated in the City of Cambridge, County of Middlesex, Commonwealth of Massachusetts, being more particularly shown as "Lot FP" on a plan entitled "Condominium Site Plan of Sierra & Tango Condominium in Cambridge, Massachusetts, Middlesex County, Scale 1" = 30', 10 January 2008, Gunther Engineering", recorded with the Middlesex S. D. Registry of Deeds as Plan No. 449 of 2008.

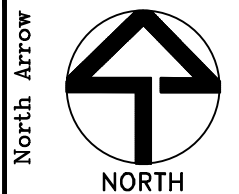
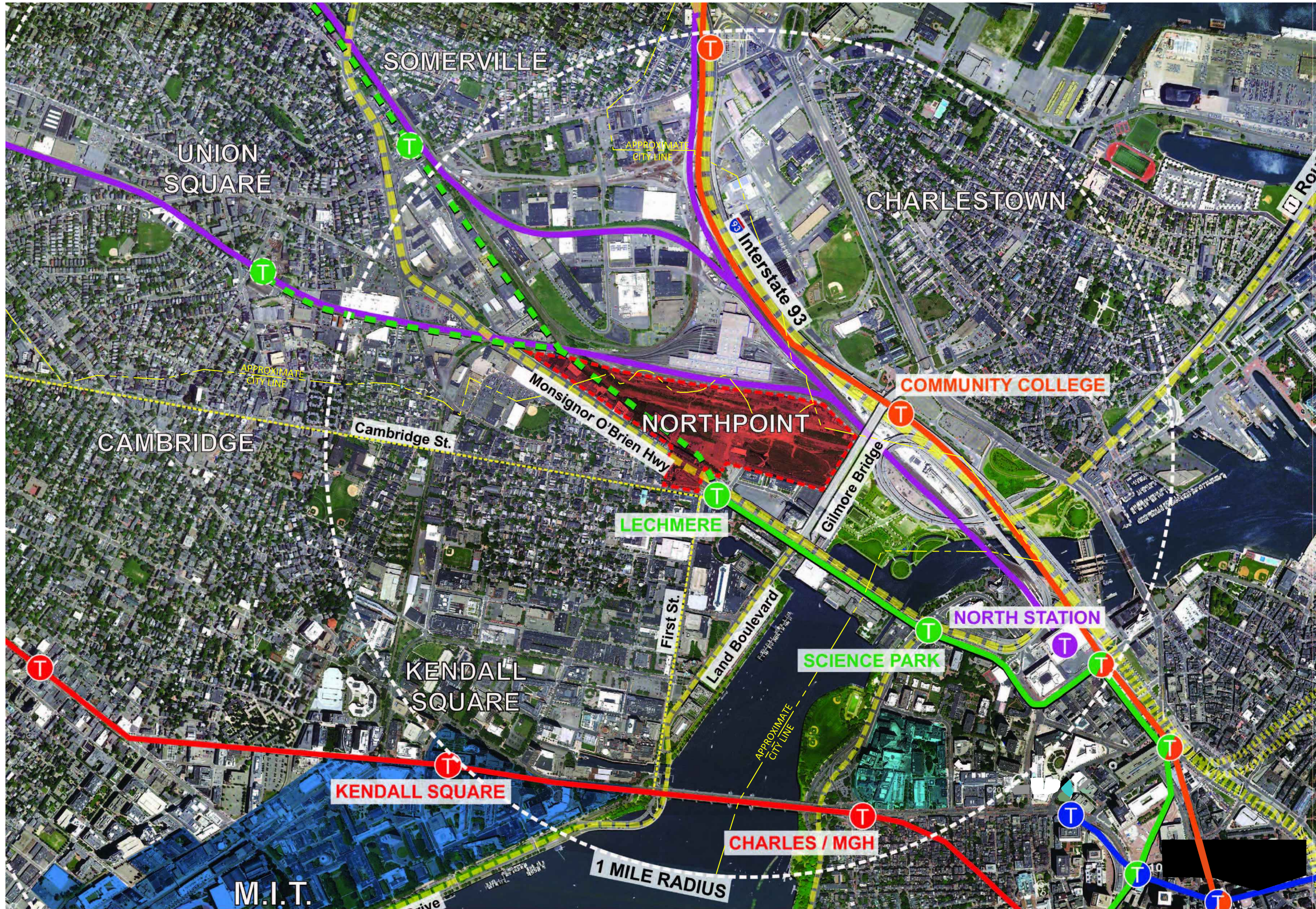
PARCEL THREE:

A certain parcel of land situated partly in the City of Cambridge and partly in the City of Boston Commonwealth of Massachusetts, Counties of Middlesex and Suffolk, located on Charlestown Avenue and being shown as Parcel 2 on a plan entitled "North Point – 'Central Park' Parcel, Subdivision Plan of Land in Boston, Cambridge and Somerville, Massachusetts, Suffolk and Middlesex Counties" dated August 16, 2010, prepared by Gunther Engineering, recorded in Middlesex County Registry of Deeds as Plan 597 of 2010 and recorded in Suffolk County Registry of Deeds in Plan Book 2010 Pages 270 and 271.

The foregoing parcels are conveyed together with the benefit (subject to the terms and provisions of the documents referenced below) of the following:

- A. Easement rights that are appurtenant to the above-described PARCEL THREE and that were reserved by Boston and Maine Corporation in paragraph 2 of that certain Release Deed and Grant of Easement from Boston and Maine Corporation to Massachusetts Bay Transportation Authority, dated July 1, 1992, and recorded with the Suffolk County Registry of Deeds in Book 17577, Page 179 and with Middlesex South District Registry of Deeds in Book 22186, Page 479, as affected by Confirmatory Release Deed and Grant of Easement dated July 20, 1993 and recorded with Suffolk Registry of Deeds in Book 19013, Page 1 and with Middlesex South District Registry of Deeds in Book 24467, Page 562, which easement rights are in the property shown as "B&M Access Easement" on a plan recorded with the Suffolk County Registry of Deeds as Plan 17577, Page 179 and Middlesex County Southern District Registry of Deeds as Plan 546 of 1992.
- B. Easements to use and access the "Developer Crossover Easement Area", granted in and subject to the terms and provisions of that certain Northpoint Parking Easement Agreement by and between CJUF III Northpoint LLC and the Massachusetts Bay Transportation Authority, dated as of February 23, 2015, and recorded in Middlesex South District Registry of Deeds in Book 65048, Page 251.

- C. Easements set forth in that certain Easement Agreement by and between North Point Apartments Limited Partnership, Archstone North Point II LLC, AVB Maple Leaf Apartments Limited Partnership, and CJUF III Northpoint LLC, dated as of July 31, 2015, and recorded in Suffolk in Book 54872, Page 23, and in Middlesex South District Registry of Deeds in Book 65856, Page 233.
- D. Easements covenants, conditions and restrictions set forth in that certain Northpoint Agreement of Covenants, Easements and Restrictions by and between CJUF III Northpoint LLC, NP Parcel N Owner LLC, and the Boston and Maine Corporation, dated April 12, 2013 and recorded with Middlesex South District Registry of Deeds in Book 61625, Page 397 and recorded with Suffolk County Registry of Deeds in Book 51306, Page 54, as affected by that certain Northpoint Parcel N Deed Covenants by and between CJUF III Northpoint LLC and NP Parcel N Owner LLC dated as of April 12, 2013 and recorded with Middlesex South District Registry of Deeds in Book 61625, Page 496, as affected by that certain First Amendment to Northpoint Agreement of Covenants, Easements and Restrictions dated as of August 21, 2014, recorded in Suffolk County Registry of Deeds in Book 53465 Page 37 and in Middlesex South District Registry of Deeds in Book 64210 Page 280, as affected by that certain Second Amendment to Northpoint Agreement of Covenants, Easements and Restrictions dated as of June 12, 2015, recorded with Suffolk County Registry of Deeds in Book 54628, Page 334, and recorded in Middlesex South District Registry of Deeds in Book 65567, Page 343.
- E. Easements covenants, conditions and restrictions set forth in that certain Agreement of Covenants, Easements and Restrictions by and between CJUF III Northpoint LLC, Sierra Plus Tango LLC, as Trustee of Sierra + Tango Condominium Trust, and Boston and Maine Corporation, dated as of April 1, 2011 and recorded with Middlesex South District Registry of Deeds in Book 56683, Page 375 and with Suffolk County Registry of Deeds in Book 47761, Page 232, as amended by First Amendment dated as of December 12, 2012 and recorded with Middlesex South District Registry of Deeds in Book 60764, Page 220.



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NorthPoint
Cambridge, Somerville and Boston,
Massachusetts
DW NP PROPERTY, LLC
One Kendall Square, Suite B3201
Cambridge, Massachusetts 02139

Locus Map

Scale: 1" = 600'±
Date: 08/25/2016
B+T Drawing No. 208402P142B-001

LOT/ BUILDING DESIGN & SITE PLAN FINAL SUBMITTAL - NORTHPOINT LOT EF

DECEMBER 5, 2017

Building Type: Commercial Building

Use Category: Commercial Office & R&D And/Or Laboratory

Total Gross Floor Area: 635,372 sf.

Above Ground - 482,136 sf.

Below Ground - 153,236 sf.

Height: 150 Feet

No. Of Floors:

Below Ground - (3) Levels Of Parking

Above Ground - (9) Levels

Parking Count = 379

Regular Parking: 305

Compact Parking (8'-4" x 18'-0"): 67

Accessible Parking: 5

VAN Accessible Parking: 2

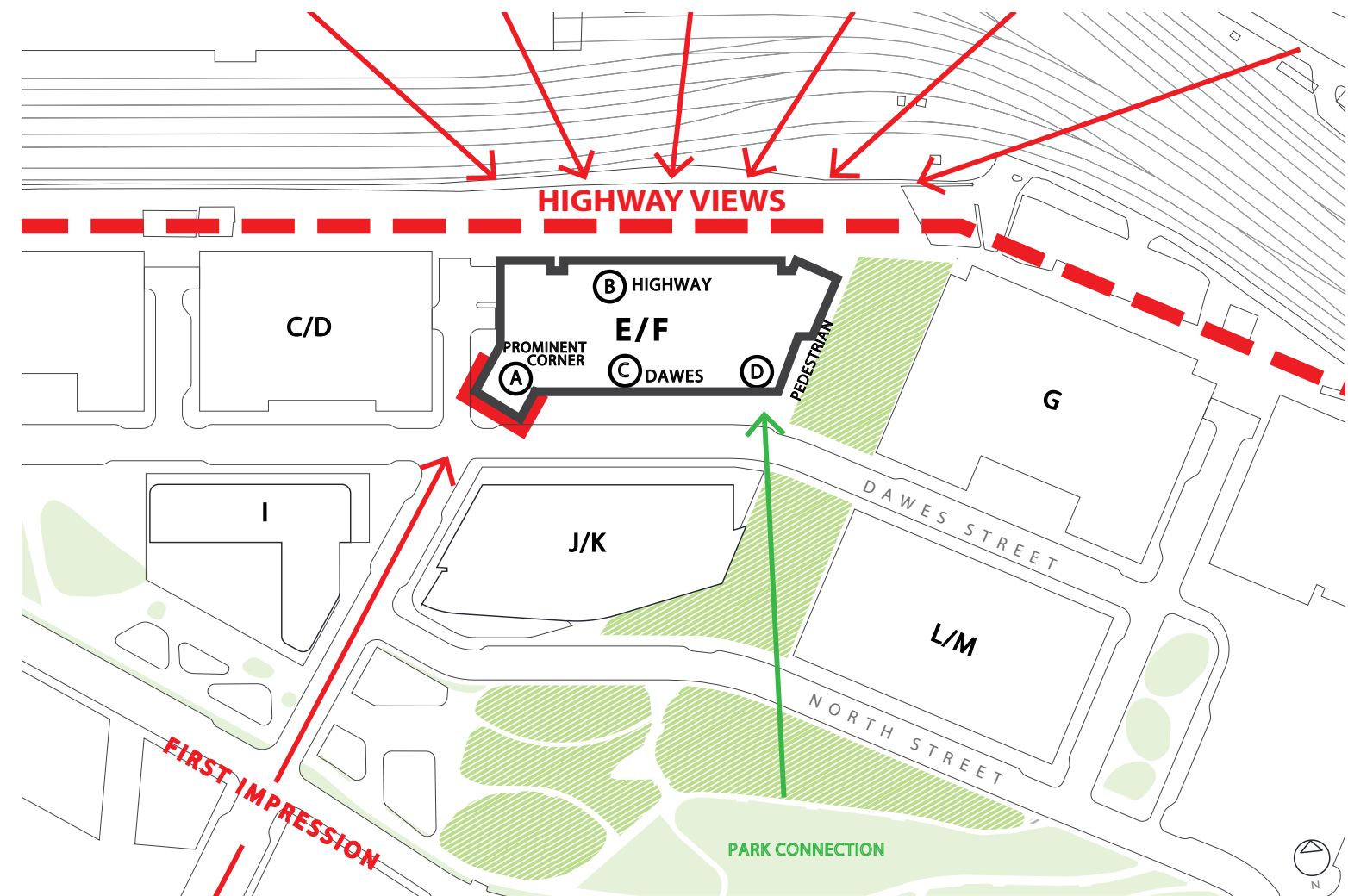
Loading Bays: 3

Design Intent

More than anything, the proposed building design for Northpoint Parcel E/F is a response to place. With several distinct acts of architecture, the building responds to the specific conditions on the site in subtle, respectful ways that result in a responsive building design that facilitates the stitching of the immediate urban condition into the greater context of the campus and ultimately, the city.

North of the building the site edges upon the MBTA train yard and the Interstate 93 corridor. In response to these 'large scale' urban elements, the North façade of the building is largely glazed and presents as a tightly composed, ordered edifice in reverence to this hyper-active urban artery. At the Northeastern corner, a section of the building is separated and turned towards the Bunker Hill Monument. These northern moves serve to situate the building with respect to a major landmark while simultaneously providing outstanding light quality and views for the users of the building. The material choices along this façade, primarily glass underscored by matte metal panel, establish an elegant, understated public face to the 50+ million yearly users of the Interstate 93 corridor.

As the building encounters the localized public realm and looks toward the Northpoint campus, decisions were made to establish a discourse with the neighborhood and its specific urban condition. To the east along Northern Baldwin Park, users are provided with a terrace at the second level which overlooks the Park and serves to enliven the urban environment. Along the front south facing façade, the building becomes transparent at the street level. In some cases, the interior/exterior boundaries are blurred altogether through the implementation of operable walls, literally opening to the outside and establishing a direct conversation between the users and the public. The façade is activated, retail amenities are situated here, as well as ample outdoor seating and vegetation. Above, material along these public facing facades brings warmth and earthen color to the campus through its terra-cotta and limestone expression. Fenestration is treated at the urban/pedestrian scale, sized down to seamlessly mesh with the fabric of the public realm. Congruously, at the Southwestern corner, a piece of the building is separated and turned to face First Street. Here, the building is raised into a glazed limestone tower above the primary building mass and becomes a beacon, acting as a campus landmark and the terminus for the major Northpoint thoroughfare.



The reflexive design addresses the site in its consideration of 'situational scale'; at the north, the high speed commuter is presented with an elegant, restrained composition free from distraction and at the south, the pedestrian is provided with opportunity to engage with the building, punctuated by the activated ground floor and its surroundings.

I.C. - ZONING CONFORMANCE REVIEW

SOMERVILLE ZONING SECTION 6.6 - NORTHPOINT SPECIAL DISTRICT

✓ COMPLIANT AS DESIGNED

Per definition 2.2.52.a. Facade. The exterior wall of a building oriented in whole or in part toward a front lot line

Building has one Primary Facade which is the South Facade. All calculations have been performed on the South Facade of the Building.

Building does not have a Secondary Facade

6.6.7 Lots

- ✓ A. Lots shall have a primary frontage abutting a thoroughfare and a lot width between fifty (50) feet minimum and five hundred (500) feet maximum.
The lot has one primary frontage at the south abutting Dawes Street, 374'-1" wide.
- ✓ B. One (1) building may be built on each lot.

6.6.8 Permitted Uses

A. The following building use categories and principal uses are established according to Table 6.2 Use Categories and Principal Uses.

Table 6.6.8 Use Categories and Principal Uses	
CATEGORY	PRINCIPAL USES
1. Retail and Consumer Service	a. Retail Sales b. Grocery Stores/Supermarkets c. Personal and/or Family Services d. Business Services e. Restaurant and/or Bar f. Theater g. Commercial Recreation
2. Commercial Office	a. Medical Practitioner b. Real Estate c. General Office d. Professional Services e. Studio Space f. Bank g. Mass Media Studio h. Educational/Institutional Services
3. Lodging	a. Hotel
4. R&D and/or Laboratory	a. Research & Development b. Laboratory
5. Institutional, Religious, and/or Educational Services	a. All uses qualifying under M.G.L. 40A, Section 3
6. Residential	a. Multi-Unit Residential
7. Accessory	a. Parking Garage b. Home Occupation c. Cellular, Radio, and Internet Transmission
8. Interim	a. Surface Parking Lot

- ✓ B. The establishment of any principal use and changes in use within the same category shall be permitted by right.
Commercial Office and R&D/Laboratory Principal Use
- C. Changes in use to a different category may be permitted by special permit, except changes in use to Institutional, Religious, and/or Educational Services meeting the criteria of M.G.L. 40A, Section 3 shall be permitted by-right.

- ✓ D. Principal uses not expressly authorized are prohibited.
All uses are authorized: see below. (F.2 & F.4)
- ✓ E. Drive-up and drive-through uses shall be prohibited.
- F. Use Performance Standards.
- ✓ 2. Commercial Office Use Category.
a. Medical Practitioner.
b. Real Estate.
c. General Office.
d. Professional Services.
e. Studio Space.
f. Bank.
g. Mass Media Studio.
h. Educational/Institutional Services.
- ✓ 4. R&D and/or Laboratory Use Category.
a. All Principal Uses.
i. Storage of flammable liquids, gas, or explosives for off-site use shall be prohibited.

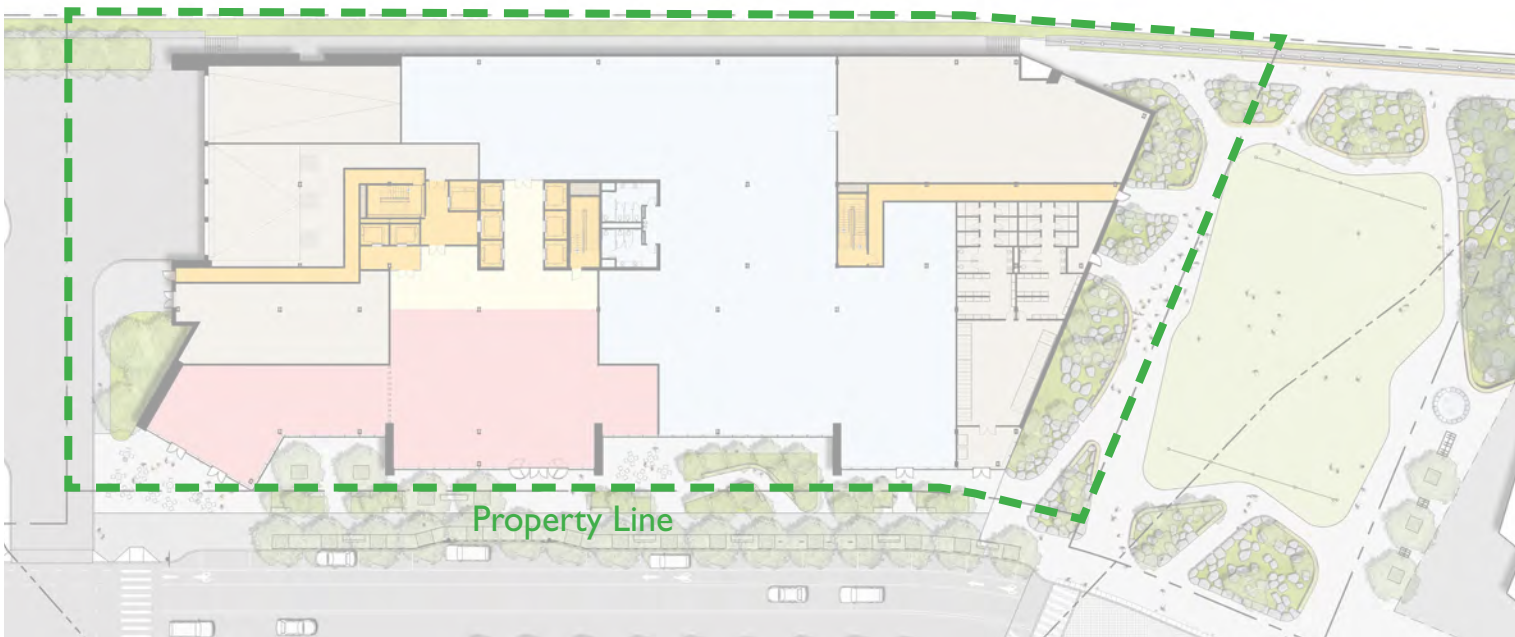
ii. Floor space dedicated to product assembly, packaging, and/or storage shall be limited to twenty-five percent (25%) gross floor area.

iii. All dust, fumes, gases, odors, smoke or vapors, noise, or vibrations shall be effectively confined to the lot.

6.6.9 Building Types

A. General.

- ✓ 1. Facades shall be built parallel to a front lot line or to the tangent of a curved front lot line.



- ✓ 2. Height limits do not apply to mechanical penthouses; cellular, radio, and internet transmission equipment; or vents or exhausts.
Building height is 150'.
- ✓ 3. The floor plate of any story shall not be larger in area than the story below.
2.2.59.a. Floor Plate. The total gross floor area of a single story of a building, excluding balconies.

D. Commercial Building.

PROCESS

a. Plan Submittal	Design and Site Plan
b. Required Permit	N/A (by right)

LOT OCCUPATION

a. Setback/Build-to Lines	
i. Primary Front	0 ft. / 20 ft. max.
ii. Secondary Front	0 ft. / 20 ft. max.

FORM

A a. Facade Build-Out	80% min.
B b. Floor Plate	50,000 sq. ft. max.
c. Height	
C i. Minimum	4 stories
D ii. Maximum	150 ft.
E d. Continuous Facade	100 ft. max.

FENESTRATION

a. Ground Floor, Primary	65% min.
b. Ground Floor, Secondary	50% min.
c. Upper Floors	30% min - 50% max.
d. Blank Wall	20 ft. max.

1. Development Standards

- a. A minimum of sixty percent (60%) of the ground floor frontage shall be occupied by active uses, including Retail and Consumer Service uses or services provided accessory to commercial, lodging, and/or R&D/ laboratory principal uses such as a cafe, fitness center, day care, and/or cafeteria open to the public.
- b. Ground floor uses shall be limited to two-hundred (200) feet of frontage for each use, with the exception that grocery stores/supermarkets may be permitted to occupy more than two-hundred (200) feet of frontage by SPECIAL PERMIT.
- c. Ground floor uses shall have individual entrances with access directly onto a sidewalk for each use.
- d. Upper story uses shall be accessed by a prominent common lobby entrance. Lobby entrances should be well-defined, clearly visible, and universally accessible from the adjacent public sidewalk.
- e. FENESTRATION shall be calculated for the wall area between two (2) feet and twelve (12) feet in height for ground floor FACADES and the total wall area of each floor for stories above the first.

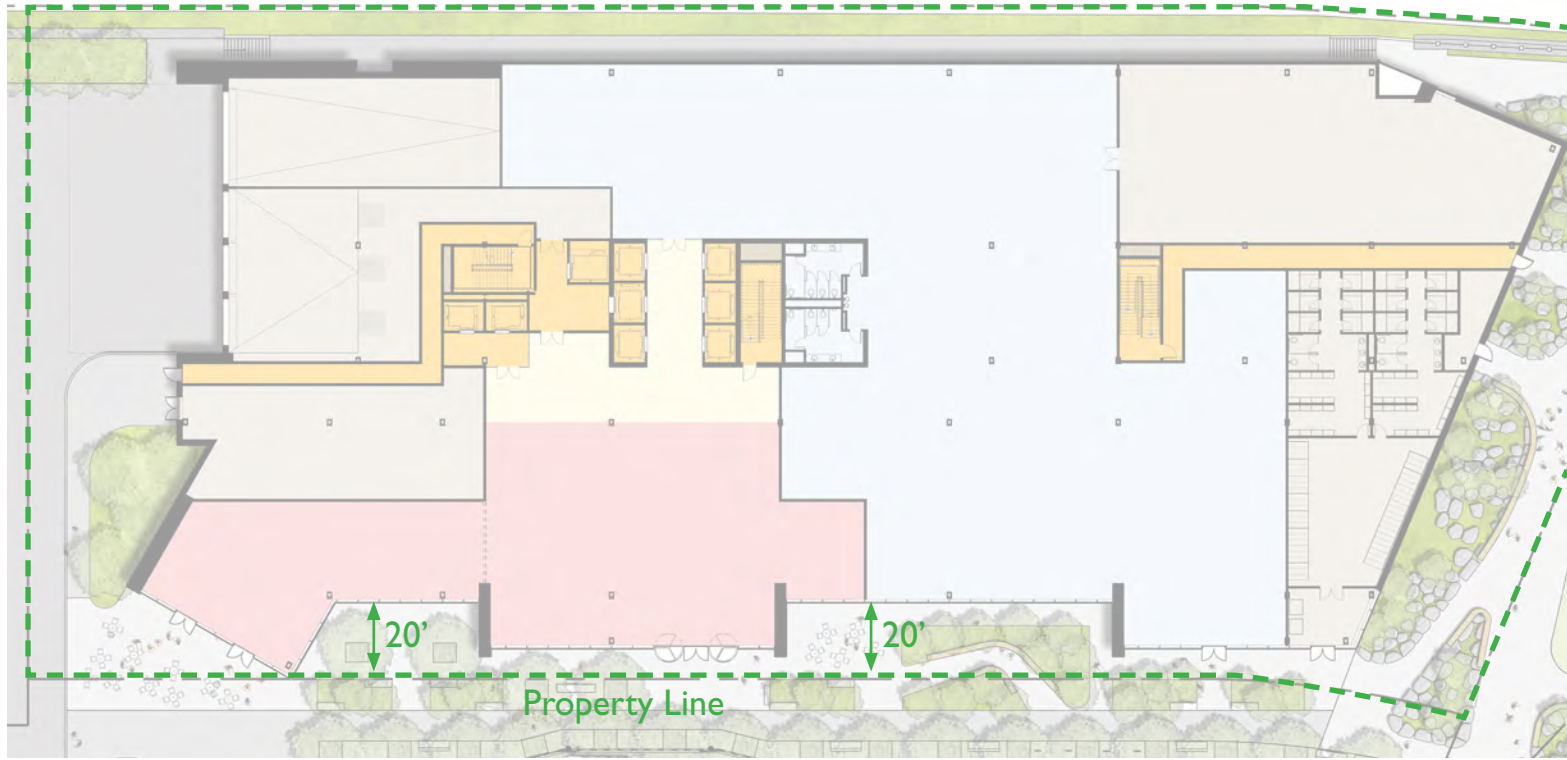
D. Commercial Building Compliance

LOT OCCUPATION

a. Setback/ Build-to Line



- i. Primary Front 0 ft. / 20 ft. max
- ii. Secondary Front 0 ft. / 20 ft. max

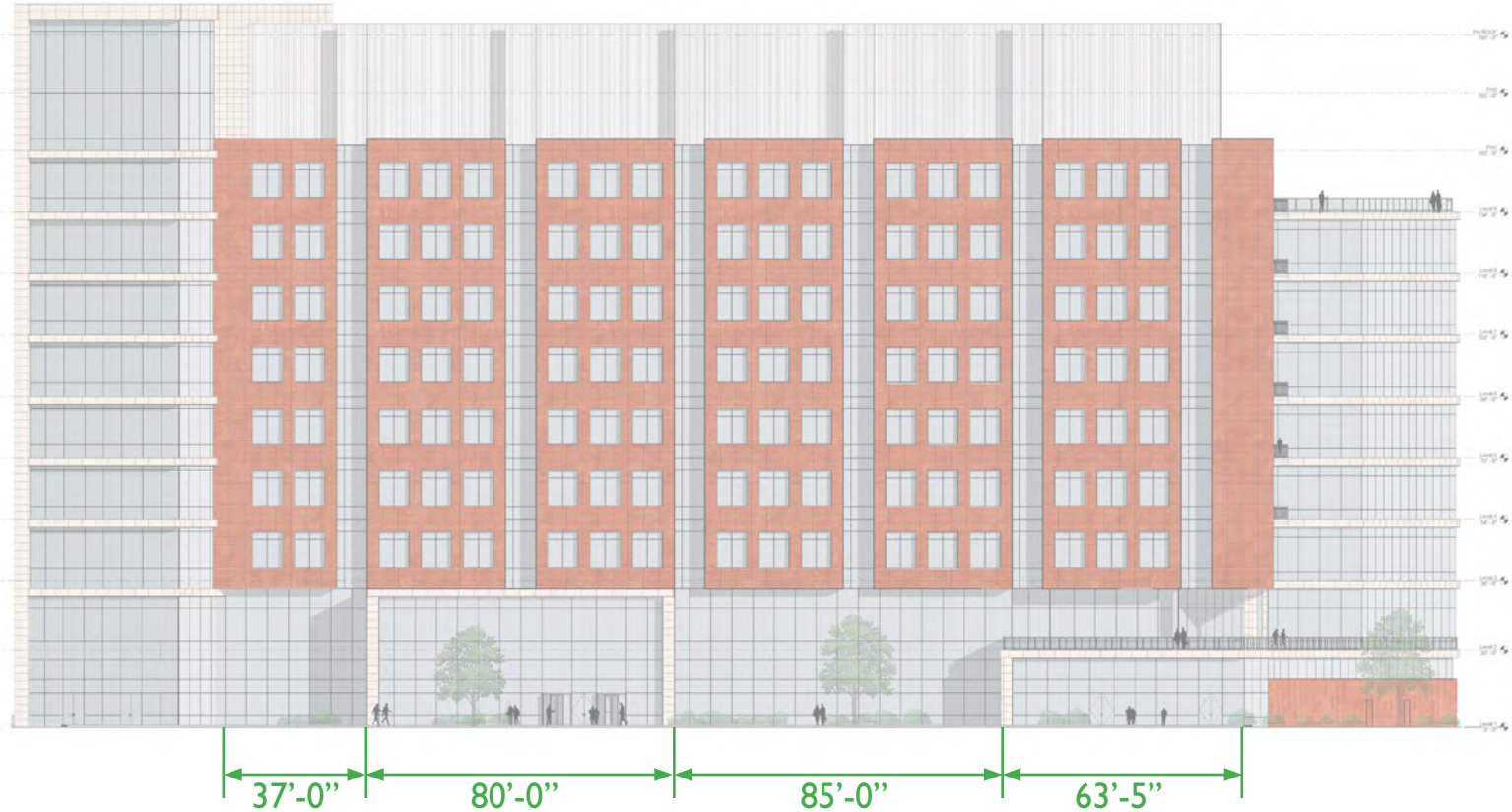


FORM

- a. Facade Build-Out 80% min 86%
- b. Floor Plate 50,000 sq.ft. max +/- 48,000 sf TYP.
- c. Height
 - i. Minimum 4 stories 9 Stories
 - ii. Maximum 150 ft. 150 ft.



- d. Continuous Facade 100 ft. max. 85 ft. max.



FENESTRATION
2.2.56.a. Fenestration. The design and placement of windows on a building.



- a. Ground Floor, Primary 65% min
- b. Ground Floor, Secondary 50% min.
- c. Upper Floors 30% min - 50% max.



- d. Blank Wall 20 ft. max
2.2.17.a. Blank wall. A portion of any facade of a building that does not include a substantial material change; windows, doors, columns, pilasters or other articulation greater than twelve (12) inches in depth.

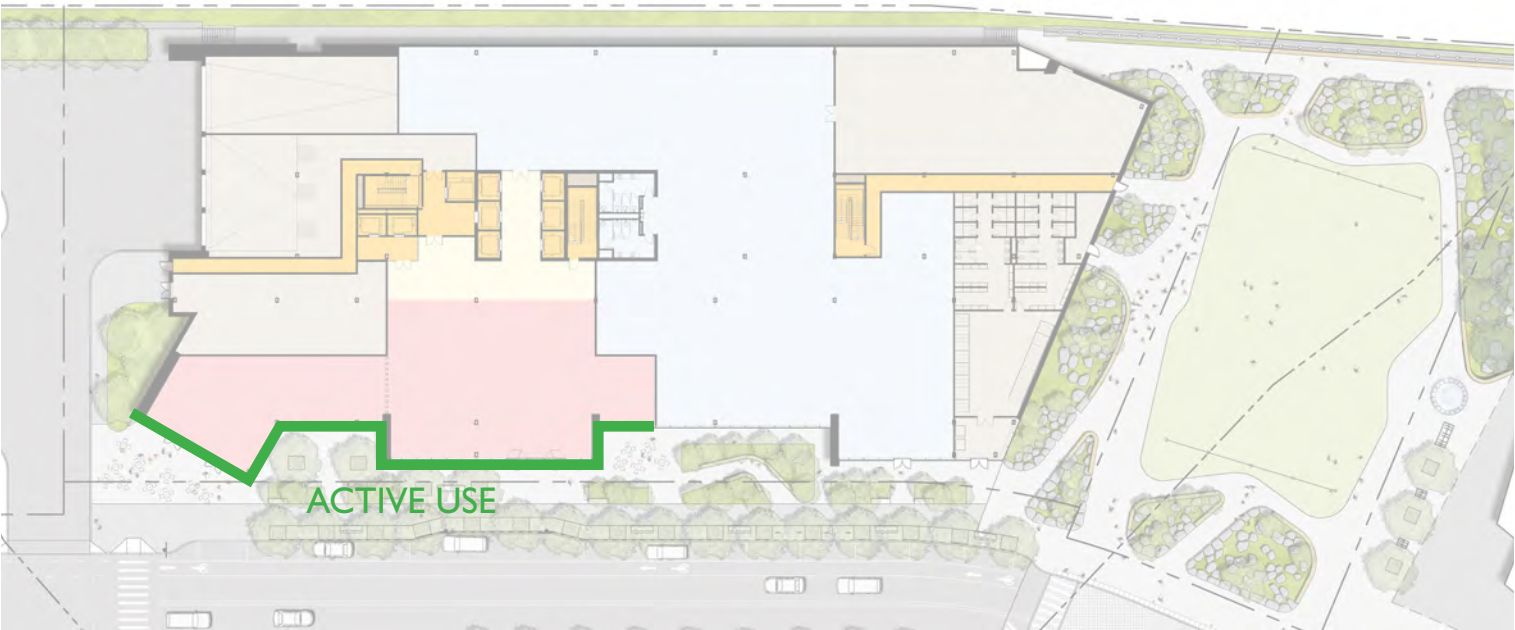


See Building Elevations Section (54-59) for illustration.

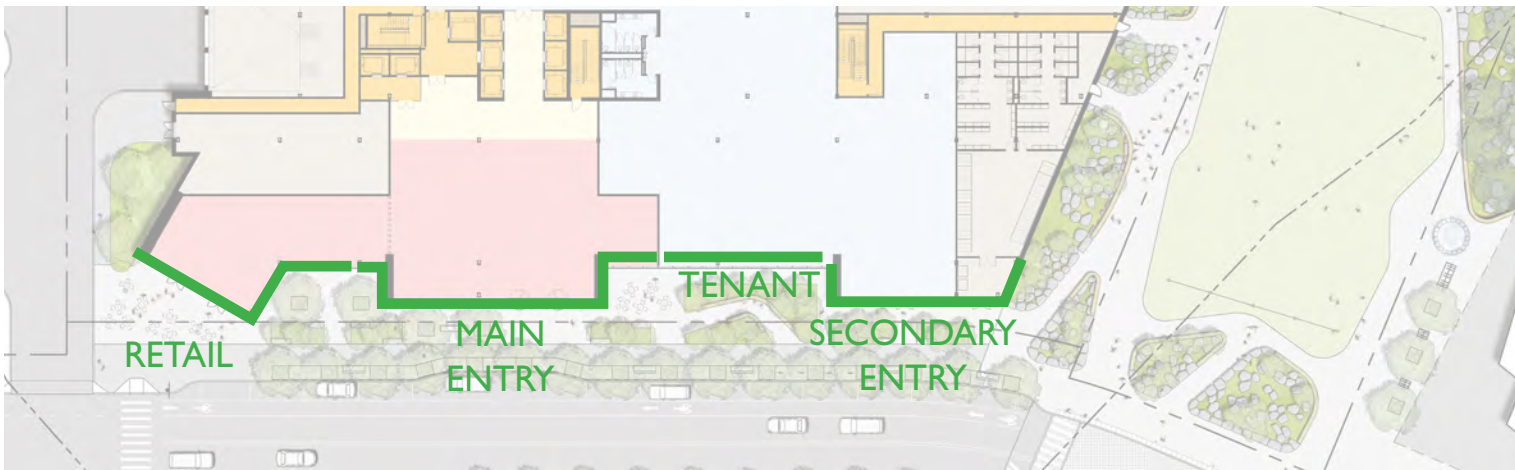
D. Commercial Building Compliance

I. Development Standards

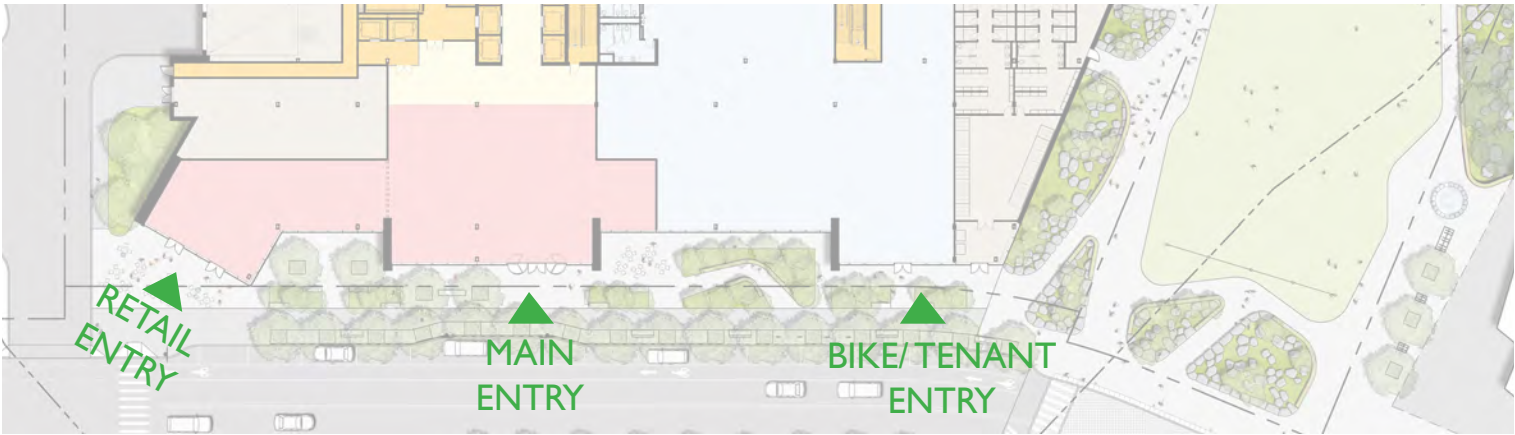
- ✓ a. A minimum of sixty percent (60%) of the ground floor frontage shall be occupied by active uses, including Retail and Consumer Service uses or services provided accessory to commercial, lodging, and/or R&D/ laboratory principal uses such as a cafe, fitness center, day care, and/or cafeteria open to public



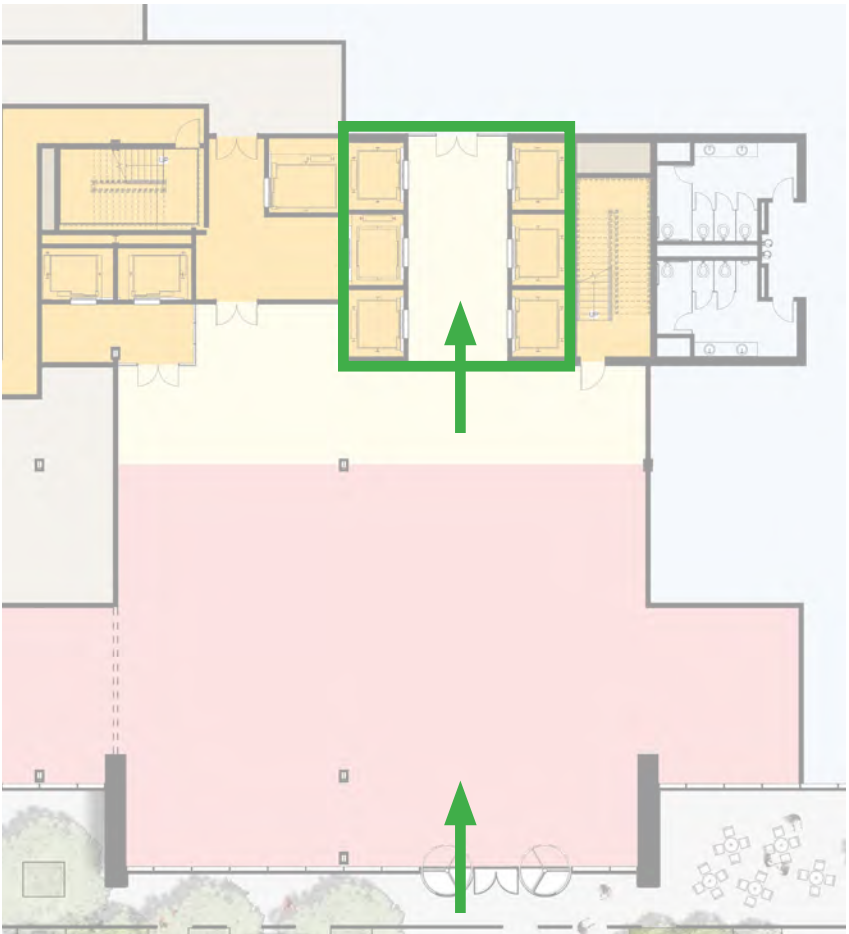
- ✓ a. Ground Floor uses shall be limited to two-hundred (200) feet of frontage for each use, with exception that grocery stores/ supermarkets may be permitted to occupy more than two-hundred (200) feet of frontage by SPECIAL PERMIT



- ✓ c. ground floor uses shall have individual entrances with access directly onto a sidewalk for each use.



- ✓ d. Upper story uses shall be accessed by a prominent common lobby entrance. Lobby entrances should be well-defined, clearly visible and universally accessible from the adjacent public sidewalk



Ground Floor Lobby Entrance



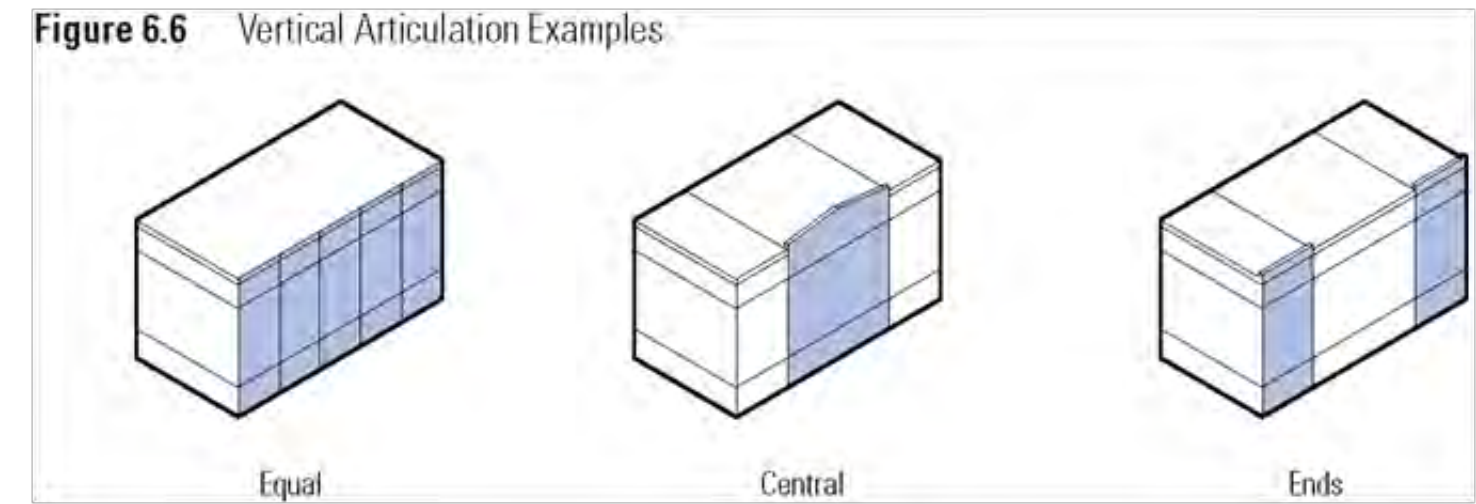
TYP. Upper Story Lobby Plan

E. Design Standards for All Building Types.

- ✓
- I. Facade Articulation.

a. Building facades shall be articulated vertically with a rhythm of bays between twenty five (25) feet and fifty (50) feet in width to create an equal, central, and/or ends focused composition as illustrated in Figure 6.6: Vertical Articulation Examples.

End focused composition with articulated bays every 44', on center.



- ✓
- b. Facades greater than one hundred (100) feet in horizontal width shall have a change in vertical plane of at least four (4) feet (in depth or projection) for at least one (1) bay in width that divides the building form into distinct massing elements that break up its apparent mass.

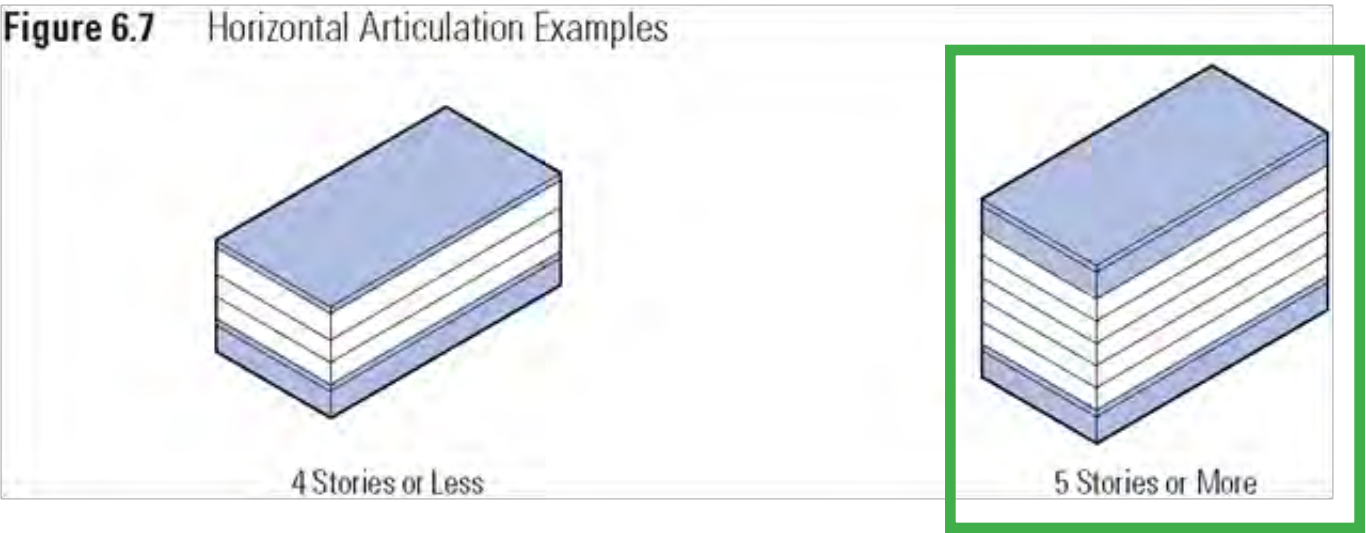
Bay projection is 4'.
- ✓
- c. Building facades shall be articulated with three clearly defined horizontal elements: a base, middle, and top (as illustrated in Figure 6.7: Horizontal Articulation Examples) according to the following:

ii. Buildings or portions of buildings (distinct massing elements) five (5) stories or more:

(a) The bottom one to three stories of a building shall be visually integrated as an expression of the building's base. The base shall be visually differentiated from the stories above by a horizontal expression line or cornice and include a change in color, building material, or pattern of fenestration.

(b) The central portion of each facade shall be visually integrated as an expression of the building's middle. The middle shall be visually differentiated from the base and top by a by a horizontal expression line or cornice and include a change in color, building material, or pattern of fenestration.

- ✓
- (c) The top one (1) to three (3) stories of each facade shall be visually integrated as an expression of the building's top. The top shall be visually differentiated from the stories below by a horizontal expression line or cornice; shall include a change in color, building material, or pattern of fenestration; and shall have a cornice, parapet, roof element, or change in massing to cap the composition.




See page 59 for Illustration

- ✓
2. Fenestration.

a. All openings, including but not limited to doors, balconies, and windows shall be square or vertical in proportion, excluding windows for first floor Retail and Consumer Service or Commercial Office uses.

b. Each horizontal element of a building (base, middle, and top) shall have a fenestration pattern that is aligned vertically and horizontally to provide order and structure to the composition. The fenestration pattern may differ between the base, middle, and top.

See page 58-59 for Illustration
-  **DIVCOWEST.**
Real Estate Investments
- T S O I / K O B U S & A S S O C I A T E S**
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- 8

6.6.10 Parking and Loading

- ✓ A. Motor Vehicle Parking.
1. Consistent with the regulatory standard for urban areas and due to close proximity to two (2) rapid transit stations, access to nearby public parking facilities, and a mixture of principal uses that permits the activities of daily life to occur within walking distance of all residences and workplaces within the NPSD, individual uses and development proposals will not be required to provide off-street motor vehicle parking, but may voluntarily elect to provide off-street motor vehicle parking according to Table 6.6.10: Parking Requirements.
(Max.Vehicle Parking = 516, Vehicle Parking Provided = 379)
- ✓ B. Bicycle Parking.
2. Sheltered, long-term bicycle parking shall be provided internal to a building for all other use categories as specified on Table 6.6.10: Parking Requirements.
(Min. Bike Parking = 83, Bike Parking provided = 100)

Table 6.6.10 Parking Requirements

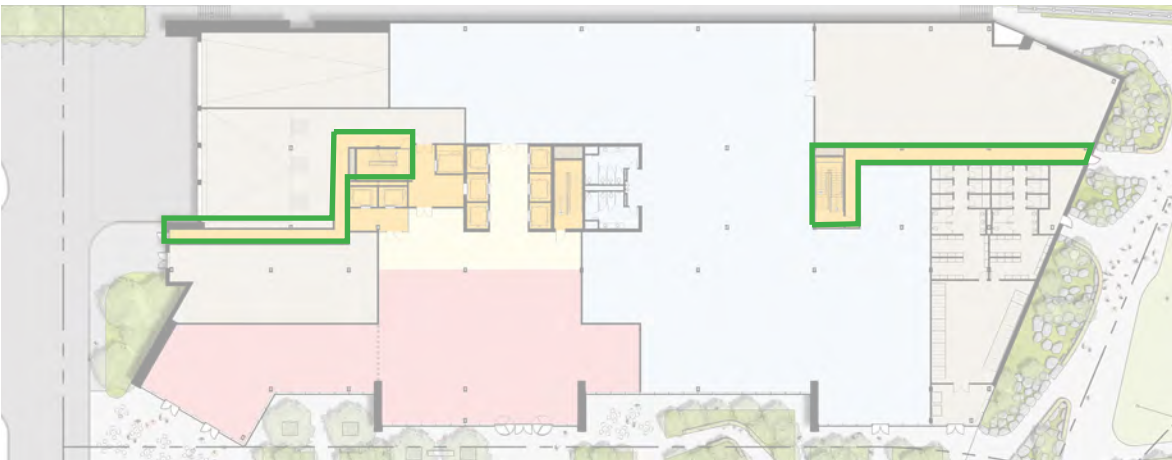
USE CATEGORY	MOTOR VEHICLE (Maximum)	BICYCLE (Minimum)
Retail & Consumer Service	.5/1,000 sq. ft.	1/1,000 sq. ft.
Commercial Office & R&D and/or Laboratory	1.25/1,000 sq. ft.	1/5,000 sq. ft.
Lodging	.5/room	n/a
Institutional, Religious, and/or Educational	1/ 4 seats of assembly	1/5,000 sq. ft.
Residential	1/unit	1 per every 2 units

- ✓ C. Parking Location.
1. All off-street parking spaces shall be located in underground parking structures, except lots abutting rail rights-of-way may have above-ground parking structures up to sixty-five (65) feet in height if utilized as an acoustic, visual, and aesthetic barrier between other uses and active rail lines, service yards, and other MBTA operations. Above-ground parking structures shall be subject to Section 6.6.10.F. **Located in underground parking structure.**
- D. Parking Management.
1. Parking spaces shall be rented, leased, or sold as an option rather and a requirement of the rental, lease, or purchase of a dwelling unit or non-residential floor space.

2. Non-residential uses shall provide employees with a transit subsidy of one hundred percent (100%), up to the maximum allowable transportation fringe benefit.

- 3. Non-residential uses shall provide ride-share matching services and preferential parking spaces for carpooling employees.
- 4. A transportation kiosk or display, providing information related to transit services in the neighborhood, shall be located within the main lobby area for each residential and nonresidential use of any building and near the main pedestrian exit of any parking garage.

- ✓ E. Parking Access.
1. A minimum of one (1) pedestrian exit from any parking lot and/or parking structure shall lead directly to a public sidewalk (i.e., not directly into a building) except underground levels which may be exited by pedestrians directly into a building. **Underground parking, see below.**
- ✓ 2. Vehicular entrances to parking lots or parking structures shall not be permitted along any primary front lot line. **None along primary front lot line.**
- ✓ 3. Vehicular entrances to parking lots, parking structures, loading docks, and service areas shall be no wider than twenty-four (24) feet along any front lot line. **0' on front lot line.**



- ✓

G. Loading Docks and Service Areas.

I. Loading docks and service areas, including trash removal, shall not be permitted along front lot lines except by special permit.



6.6.11. Environmental Performance

- A. Shadows cast by buildings shall not adversely limit at grade access to sunlight on sidewalks and civic and recreation spaces.

B. Pedestrian level wind velocities shall not exceed acceptable levels for various activities existing or proposed at particular locations.

C. Buildings shall not cause visual impairment or discomfort due to reflective spot glare and/or solar heat buildup in any nearby buildings.

D. Emissions from any parking facility and/or the heating and mechanical systems of any buildings shall not violate any state or federal ambient air quality standards.

E. Construction activities shall be carried out in accordance with the City of Somerville Code of Ordinances Chapter 11 - Public Works and other standards deemed to be appropriate by the Planning Board.

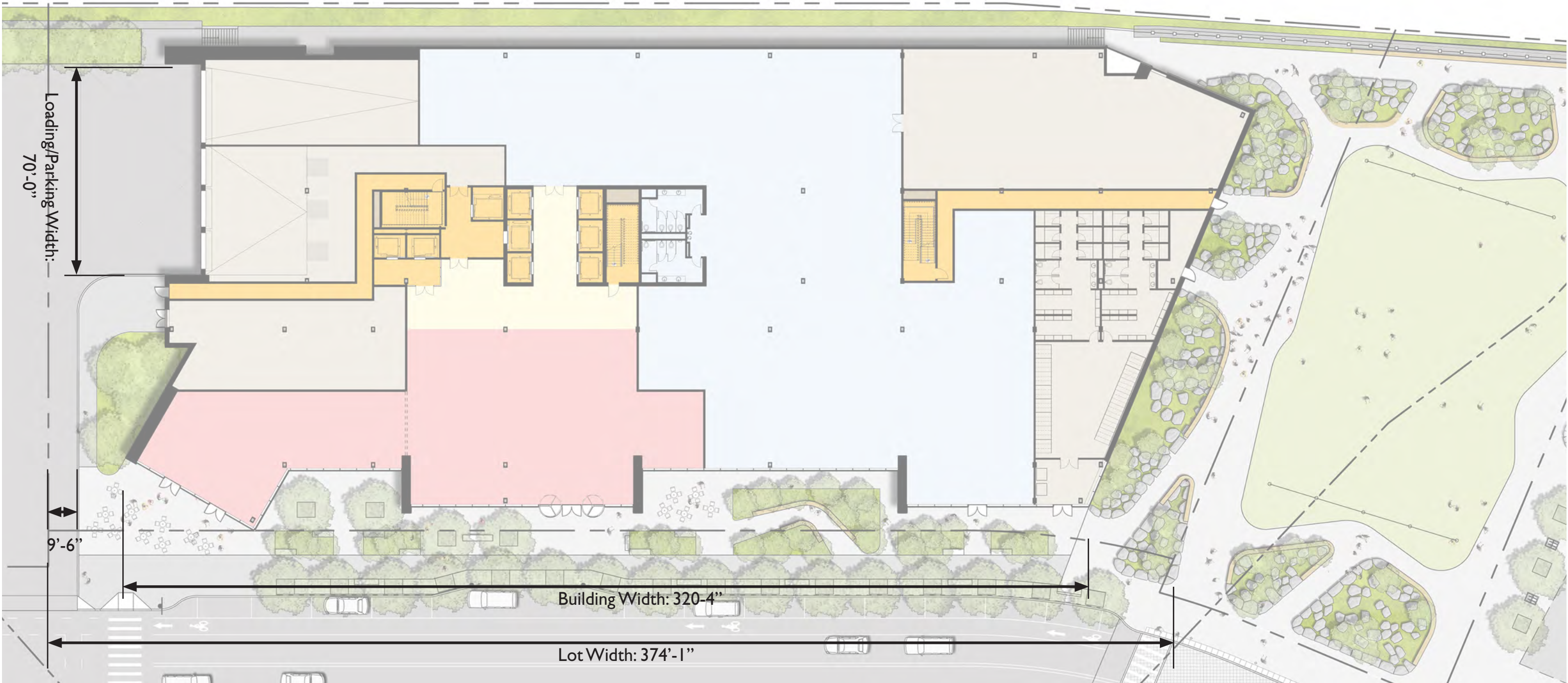
F. Private lots, parking areas, and service yards shall be screened from public view and securely protected with a temporary construction fence during all thoroughfare and site work construction. Screening and fencing shall be maintained in good condition at all times.





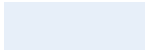


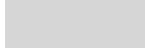
G. Wind erosion shall be mitigated and controlled through dust abatement and similar practices during site work and construction.

I.D. - ZONING CONFORMANCE STATISTICAL SUMMARY

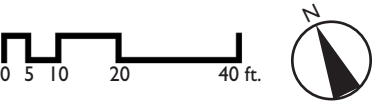
I.D.i. Facade Build-out

Required Facade Build-out = 80% min.
Designed Facade Build-out = 86%



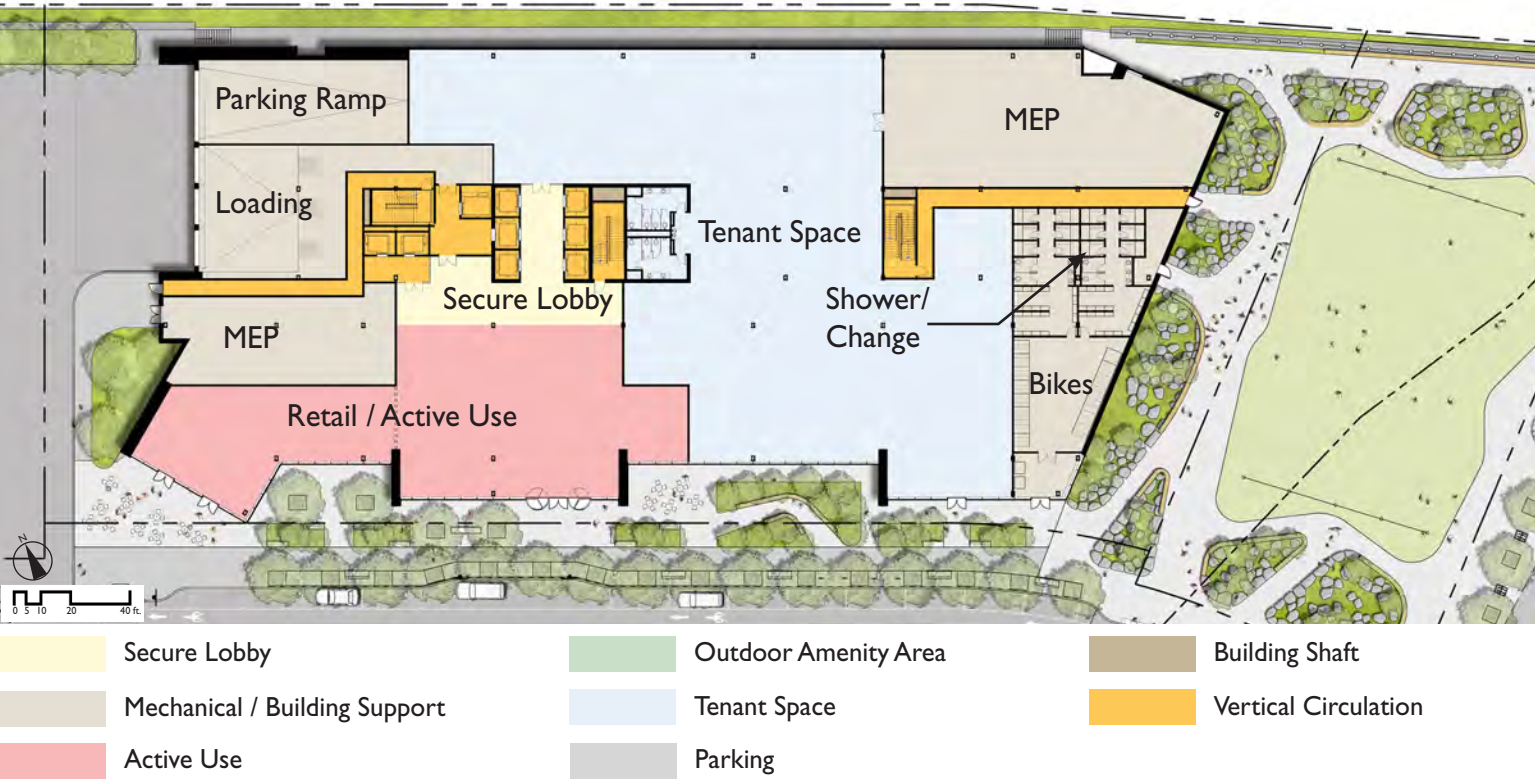
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|-------------------------------------------------------------------------------------------------------------------|------------------------------------------------------------------------------------------------------------|------------------------------------------------------------------------------------------------------------|
|  Secure Lobby |  Outdoor Amenity Area |  Building Shaft |
|  Mechanical / Building Support |  Tenant Space |  Vertical Circulation |
|  Active Use |  Parking | |

Plan Scale: 1/32" = 1' - 0"



I.D.ii. Floor Plate of Each Floor (Sq. ft.)	I.D.iii. Building Height
Penthouse Level 212,607 sf	<u>Penthouse</u> 30' - 0"
Penthouse Level 139,178 sf	<i>Penthouse Level 2</i> 15' - 0"
Floor Level 945,382 sf	<i>Penthouse Level 1</i> 15' - 0"
Floor Level 847,981 sf	<u>Height Above Ground</u> 150' - 0"
Floor Level 747,981 sf	Floor Level 916' - 0"
Floor Level 647,981 sf	Floor Level 816' - 0"
Floor Level 547,981 sf	Floor Level 716' - 0"
Floor Level 447,981 sf	Floor Level 616' - 0"
Floor Level 347,981 sf	Floor Level 516' - 0"
Floor Level 248,099 sf	Floor Level 416' - 0"
Floor Level 148,983 sf	Floor Level 316' - 0"
Parking Level 153,019 sf	Floor Level 218' - 0"
Parking Level 253,019 sf	Floor Level 120' - 0"
Parking Level 347,198 sf	
<u>TOTAL GSF</u> <u>635,372 sf</u>	<u>Below Ground</u> 33' - 0"
	<i>Parking Level 1</i> 12' - 0"
	<i>Parking Level 2</i> 10' - 6"
	<i>Parking Level 3</i> 10' - 6"

I.D.iv. Ground Floor Occupation



I.D.v. First Floor Fenestration Percentage

Required Ground Floor Primary Fenestration = 65% min
Designed Ground Floor Primary Fenestration = 69%

See Page 55 for Illustration

I.G. - PROJECT SCHEDULE

Data Date: 21-Oct-16
Run Date: 17-Nov-16 19:30
Page: 1 of 4

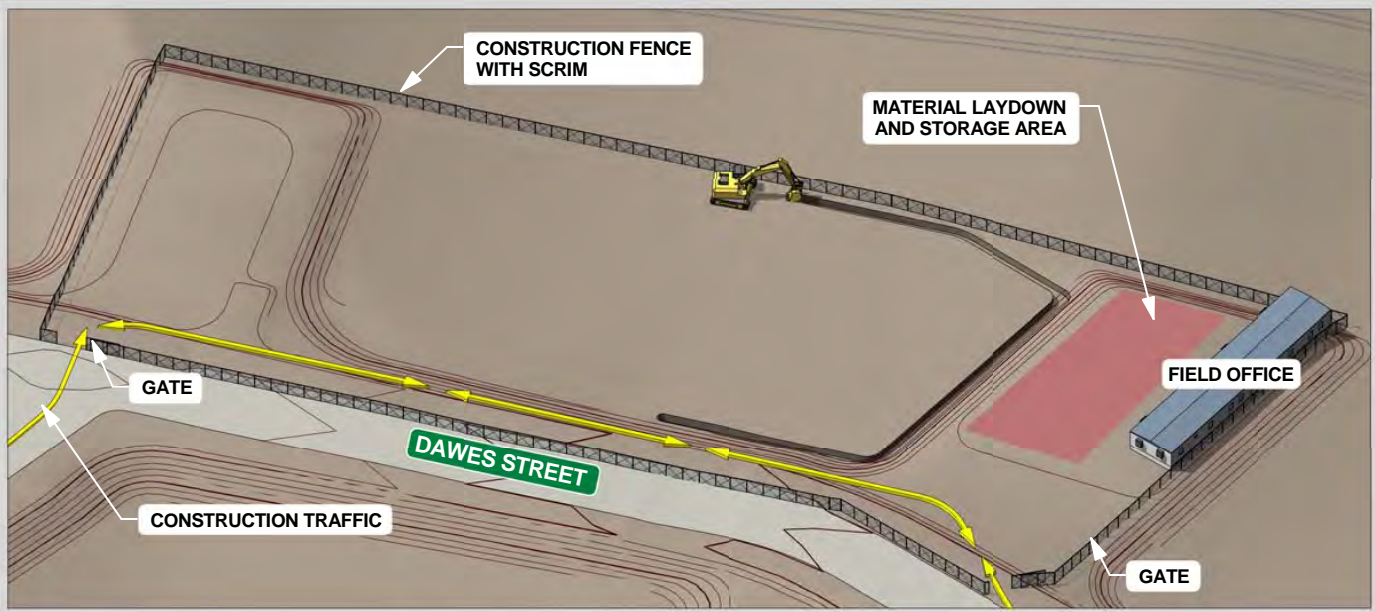


CONSIGLIERI
Est. 1905

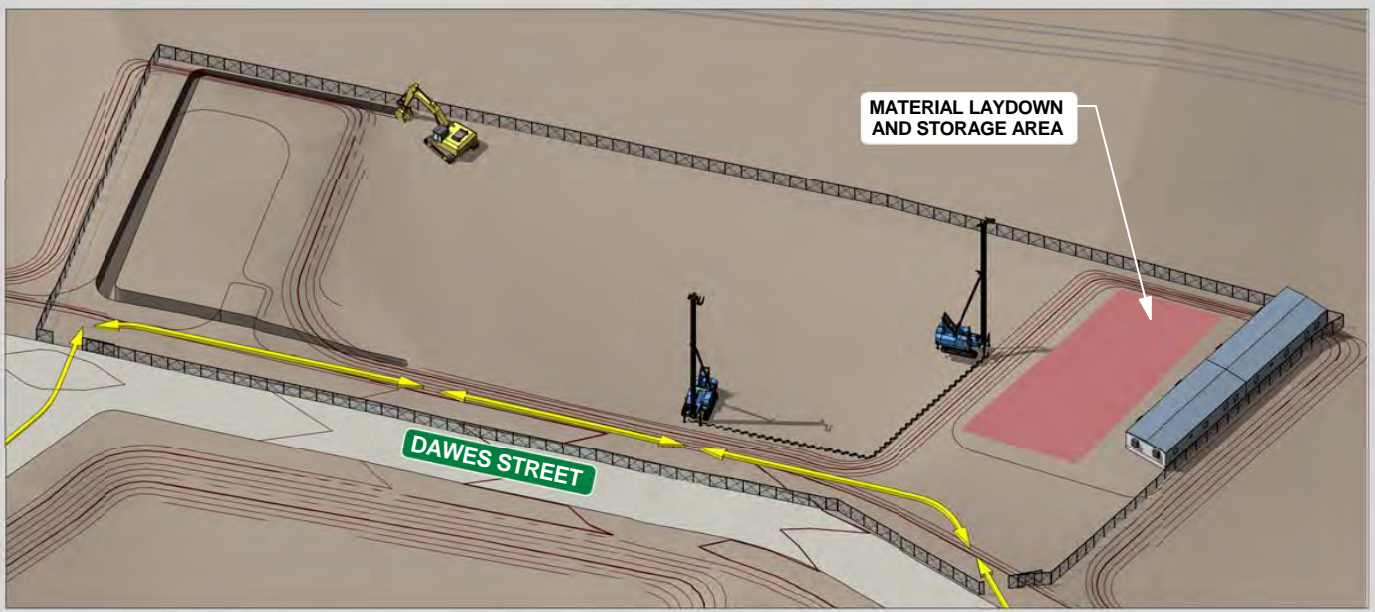
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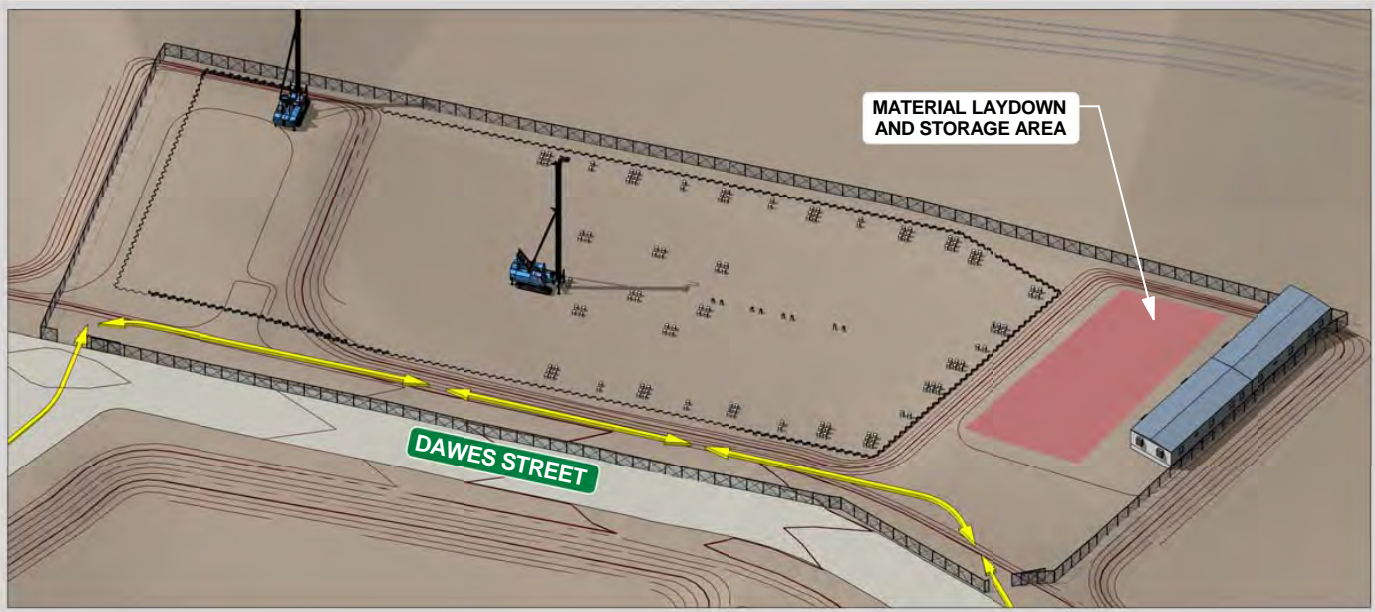
I.H. - CONSTRUCTION MANAGEMENT PLAN



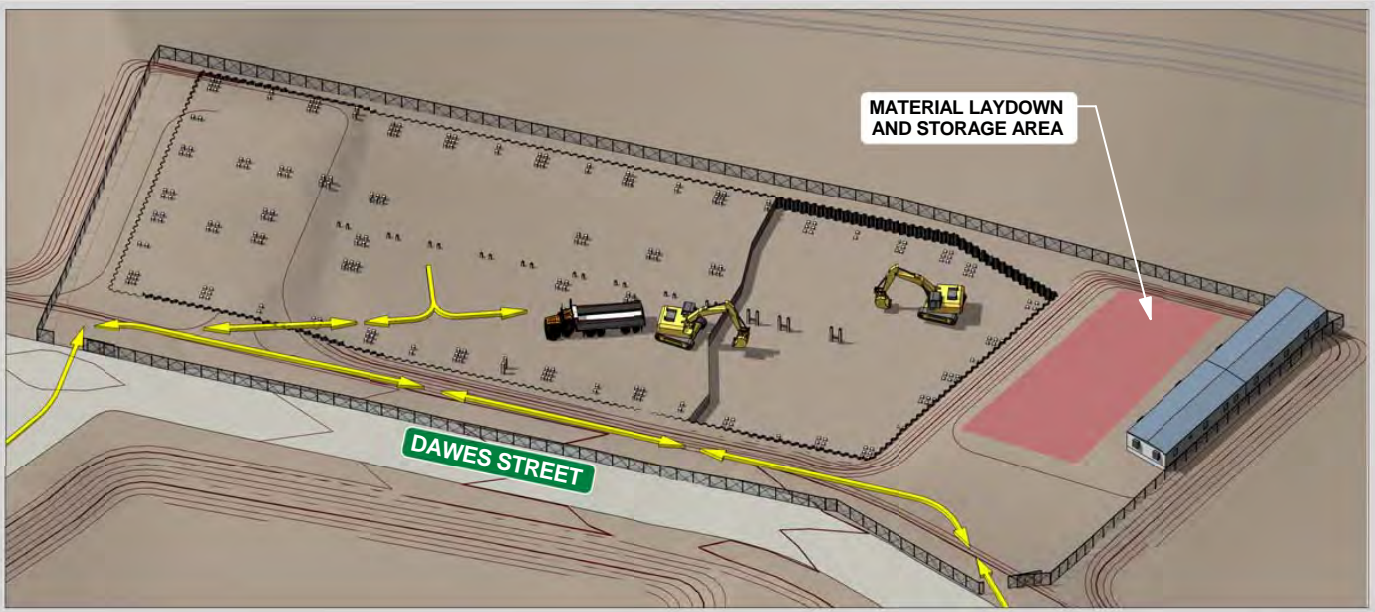
SITE MOBILIZATION AND PRETRENCHING



START OF SHEET PILES AND COMPLETION OF PRETRENCHING



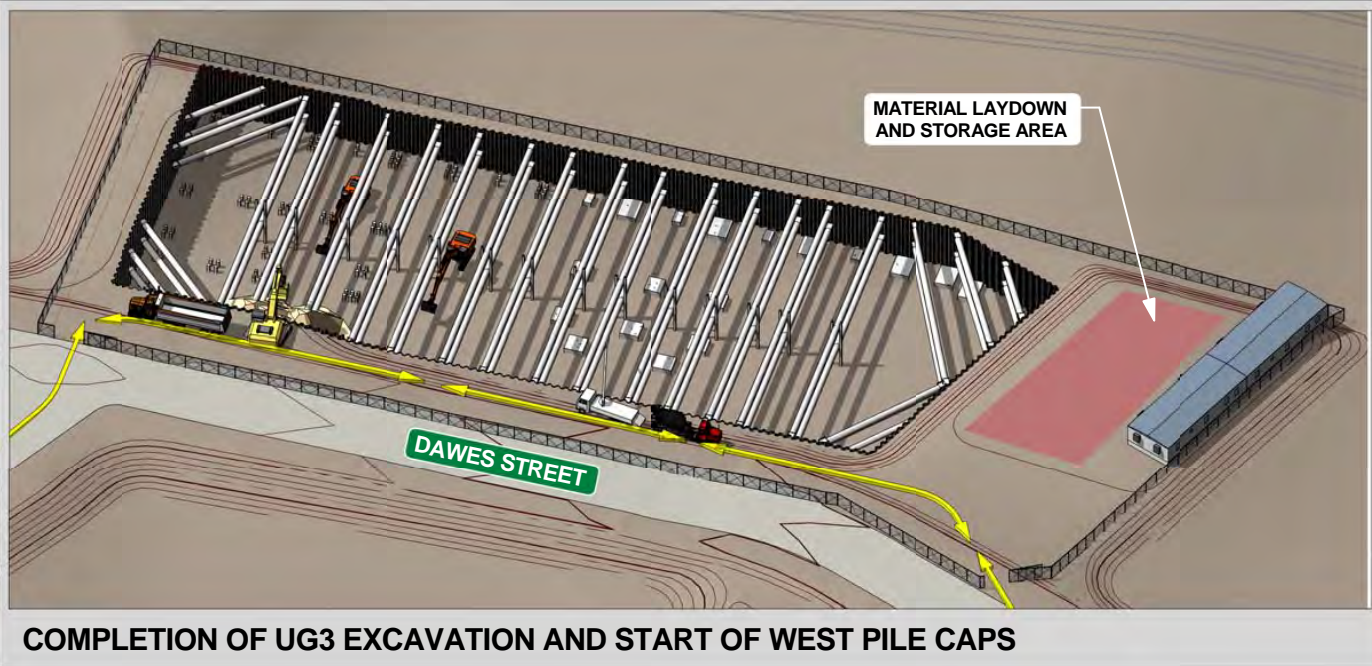
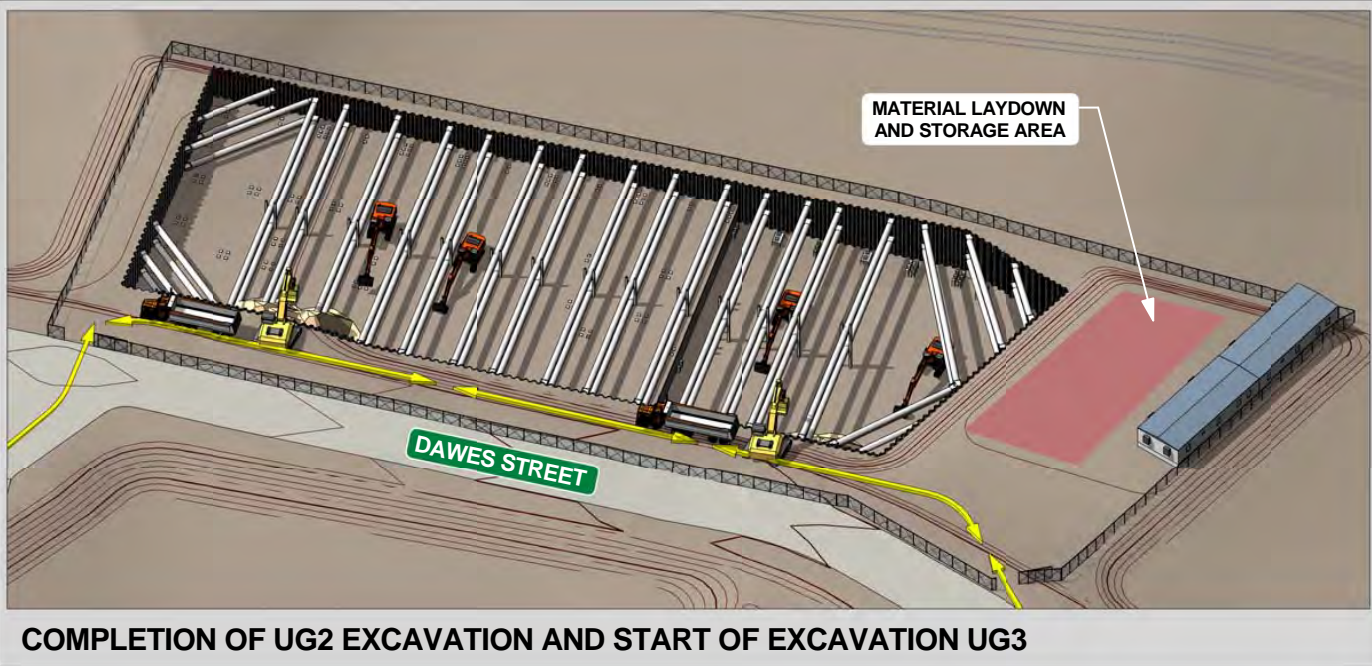
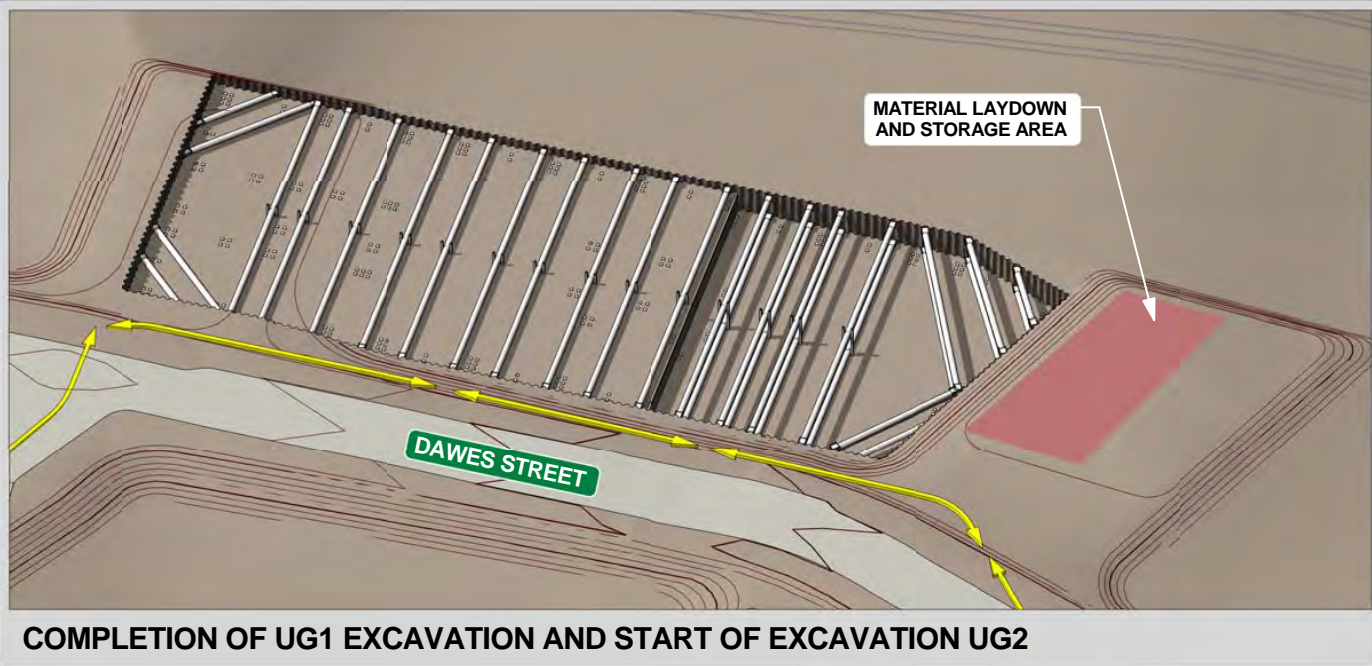
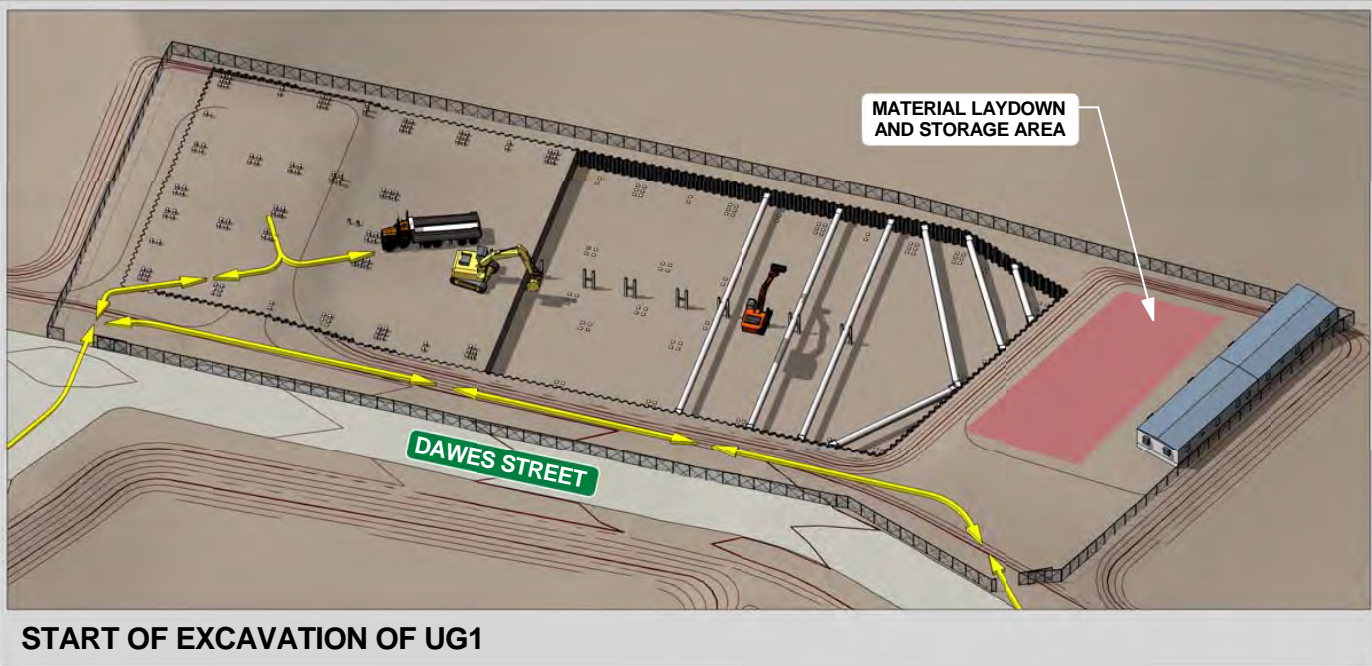
COMPLETION OF SHEET PILES; START OF STRUCTURAL PILES AND SOE PIN PILES



START OF EXCAVATION AND COMPLETION OF STRUCTURAL PILES AND SOE PIN PILES

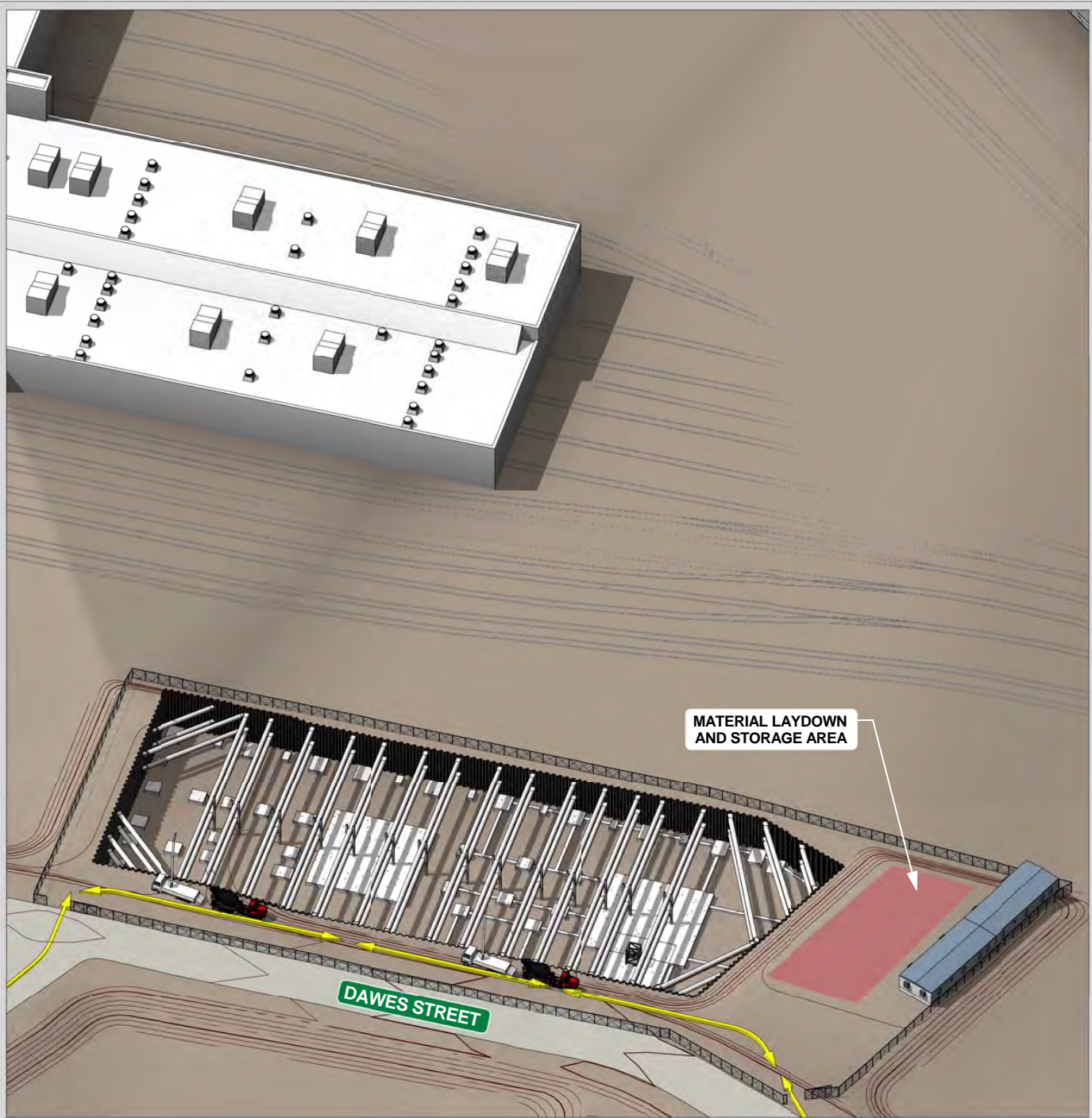
DIVCOWEST NORTHPOINT PARCEL E-F STRUCTURAL SEQUENCING



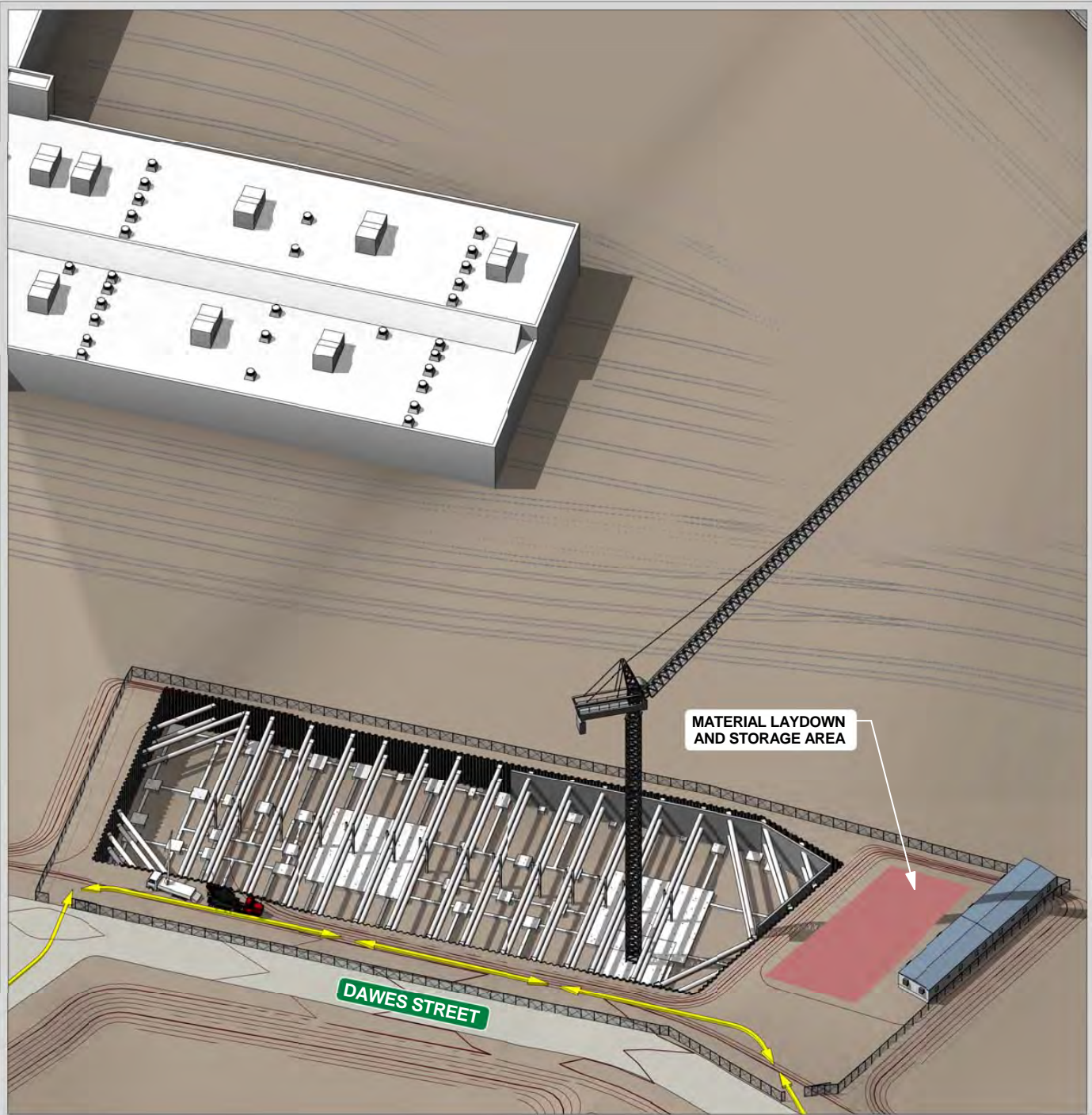


DIVCOWEST NORTHPOINT PARCEL E-F STRUCTURAL SEQUENCING





CONSTRUCTION OF EAST PILE CAPS; START OF WEST GRADE BEAMS AND WEST TOWER CRANE MAT



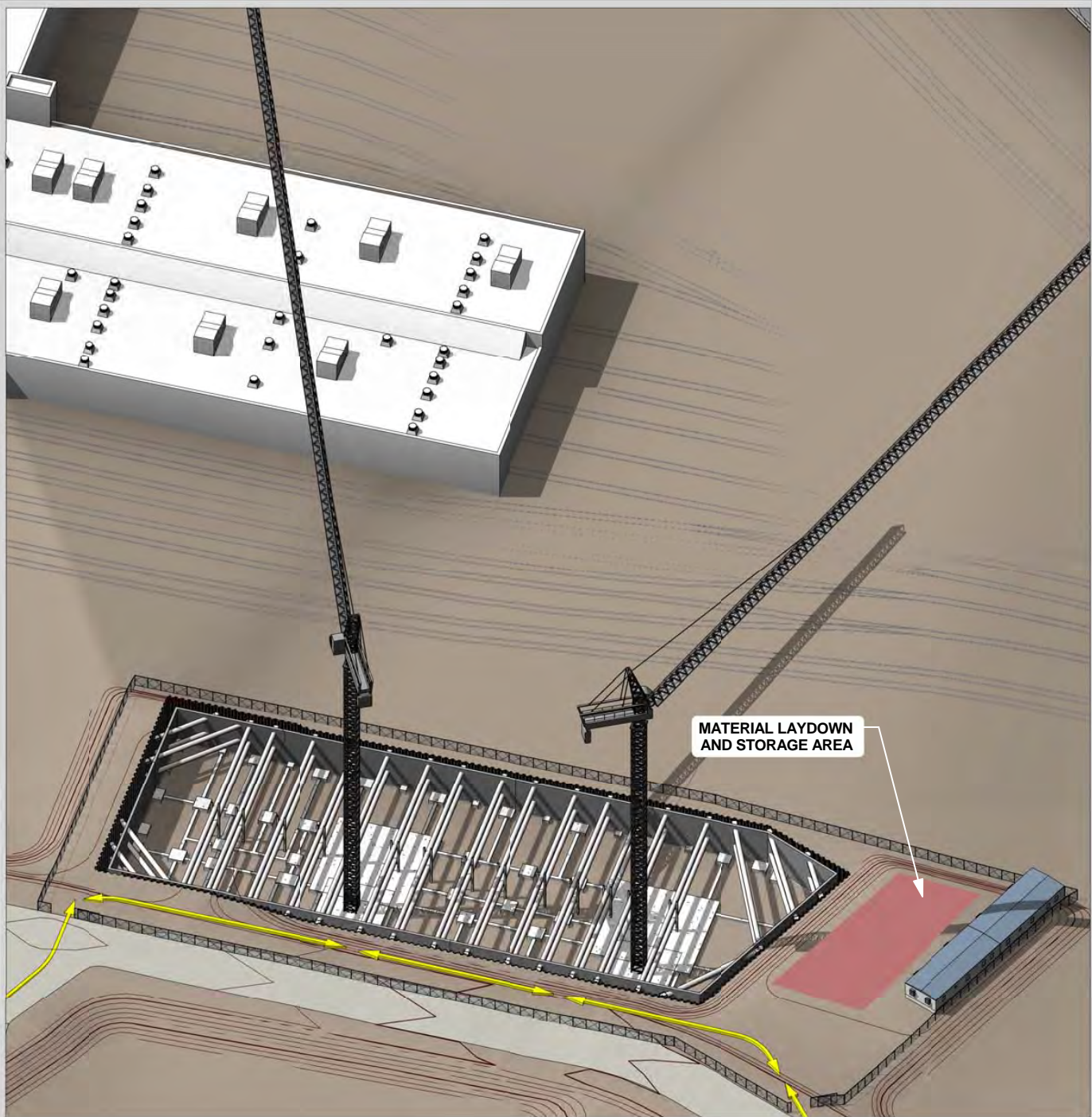
ERECTION OF WEST TOWER CRANE AND CONSTRUCTION OF WEST WALLS; START OF EAST GRADE BEAMS AND EAST TOWER CRANE MAT

DIVCOWEST NORTHPOINT PARCEL E-F STRUCTURAL SEQUENCING





ERECTION OF EAST TOWER CRANE AND CONSTRUCTION OF EAST WALLS;
COMPLETION OF WEST WALL CONSTRUCTION



COMPLETION OF EAST WALL CONSTRUCTION, START OF WEST STEEL

DIVCOWEST NORTHPOINT PARCEL E-F STRUCTURAL SEQUENCING





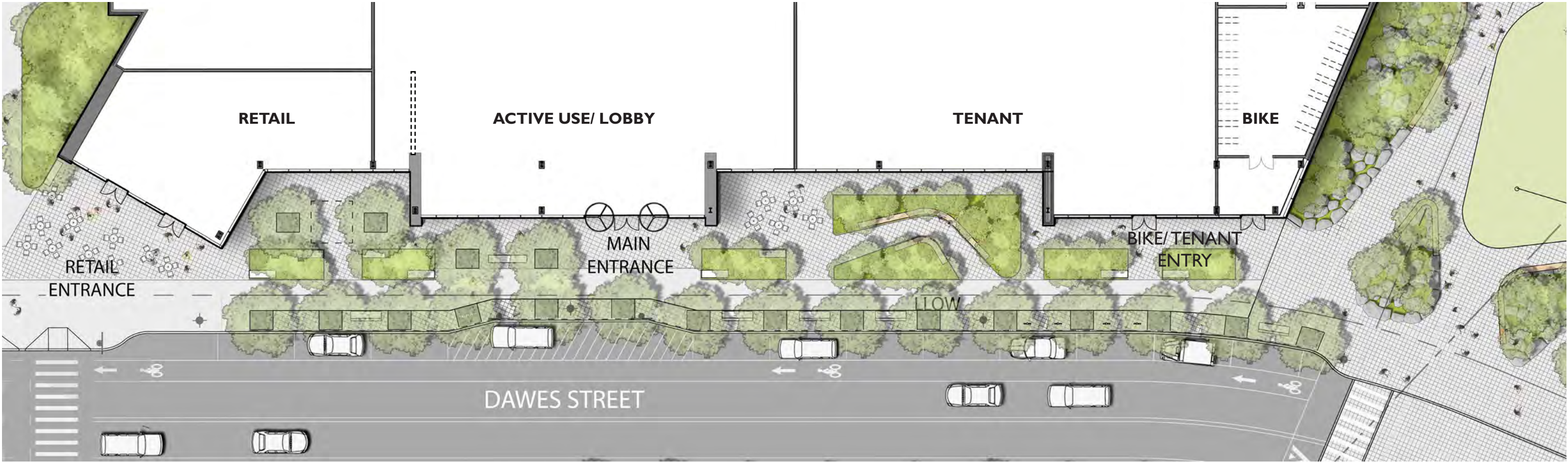
DIVCOWEST NORTHPOINT PARCEL E-F SITE LOGISTICS



2 - BUILDING INTERFACE WITH PUBLIC REALM



3 - BUILDING FRONTAGE AT FIRST FLOOR / BUILDING BASE

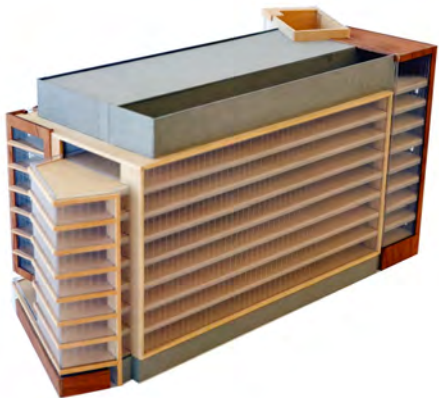
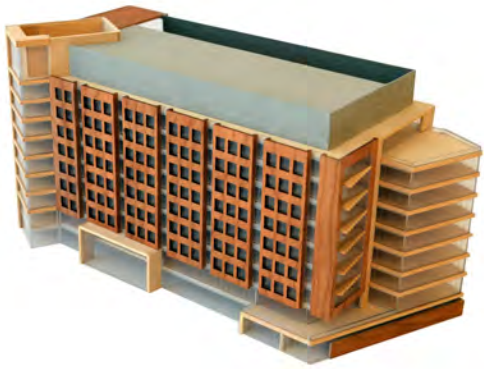


4 - RENDERED PERSPECTIVES & PHYSICAL MODEL

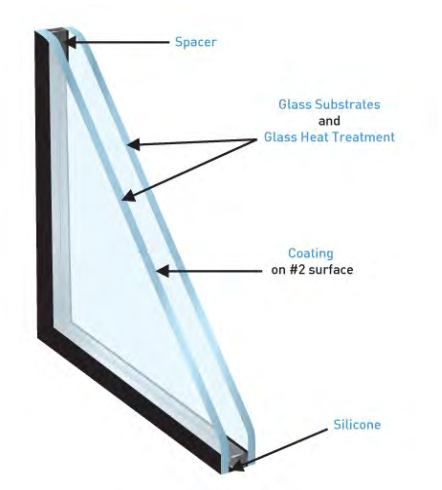
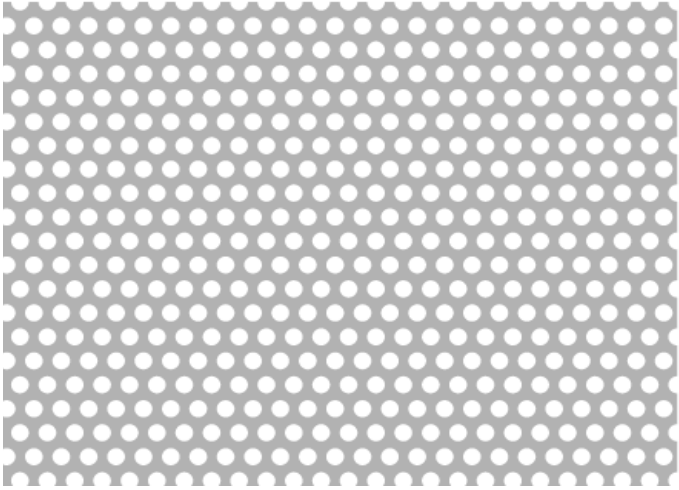
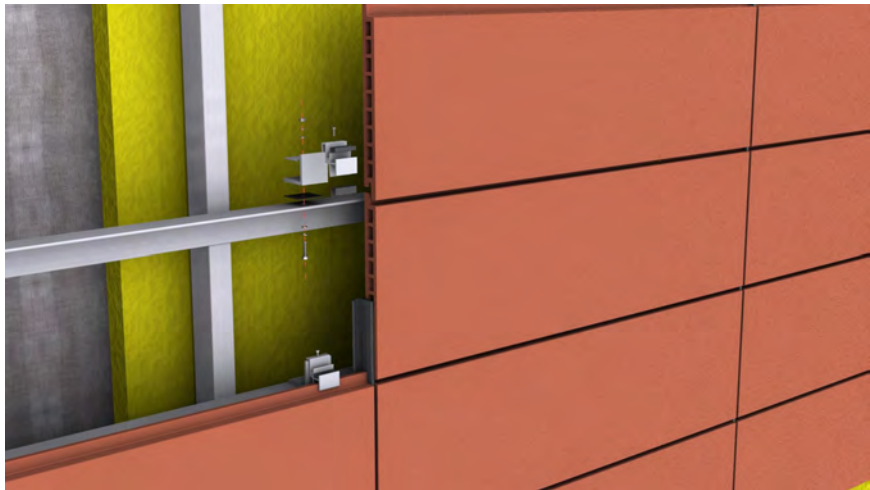
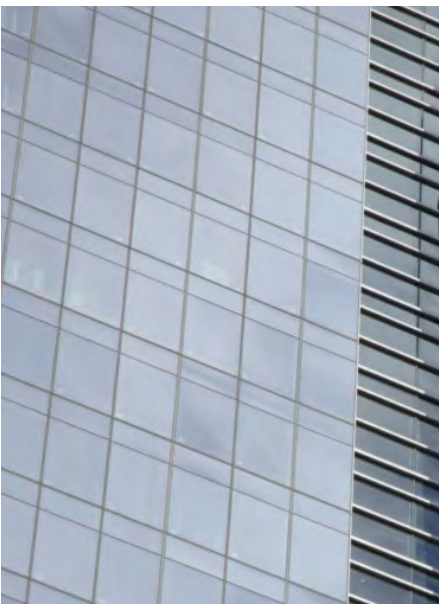








5 - BUILDING MATERIAL IDENTIFICATION



TERRA COTTA PANEL

LIMESTONE

PERFORATED METAL SCREEN

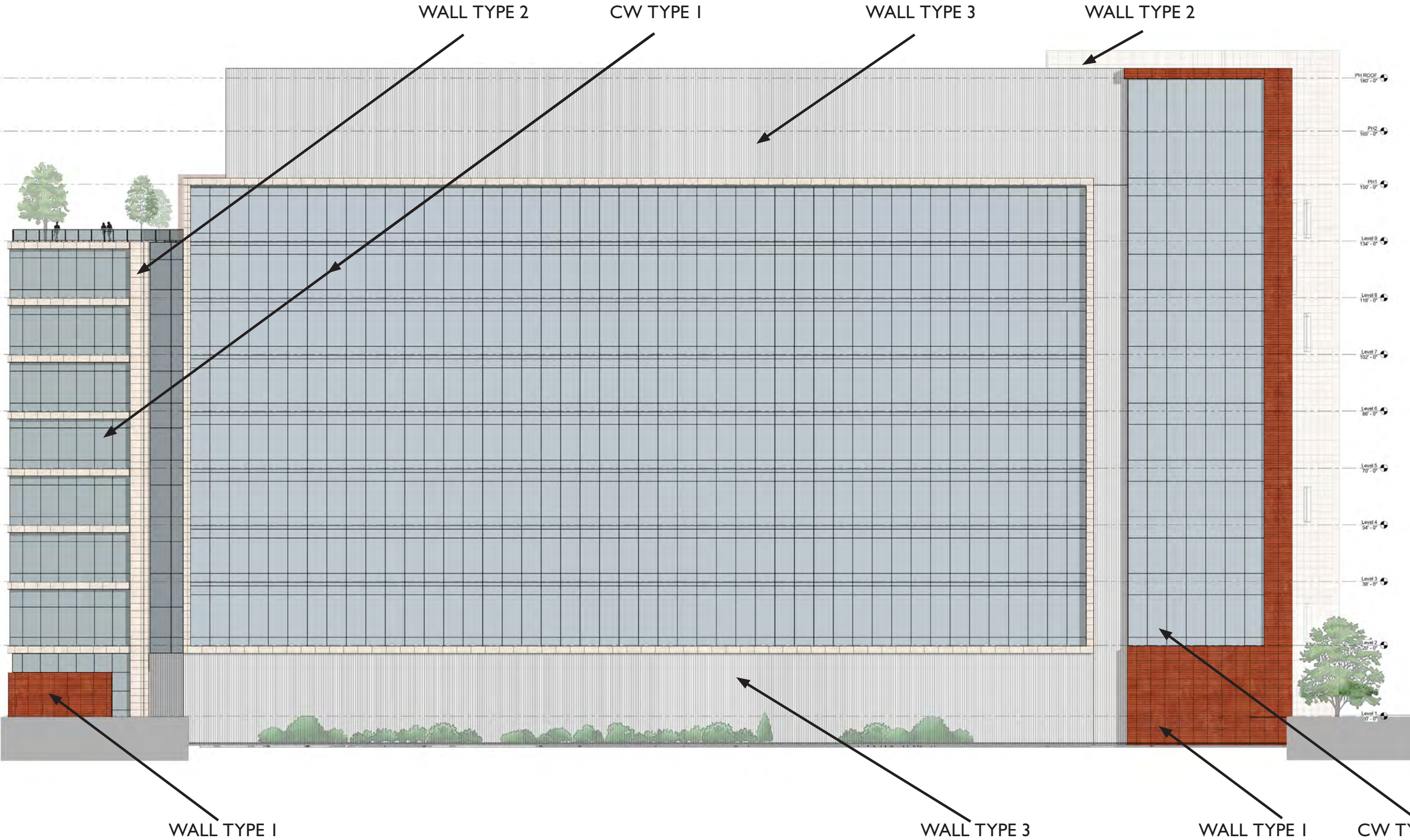
GLASS CURTAIN WALL

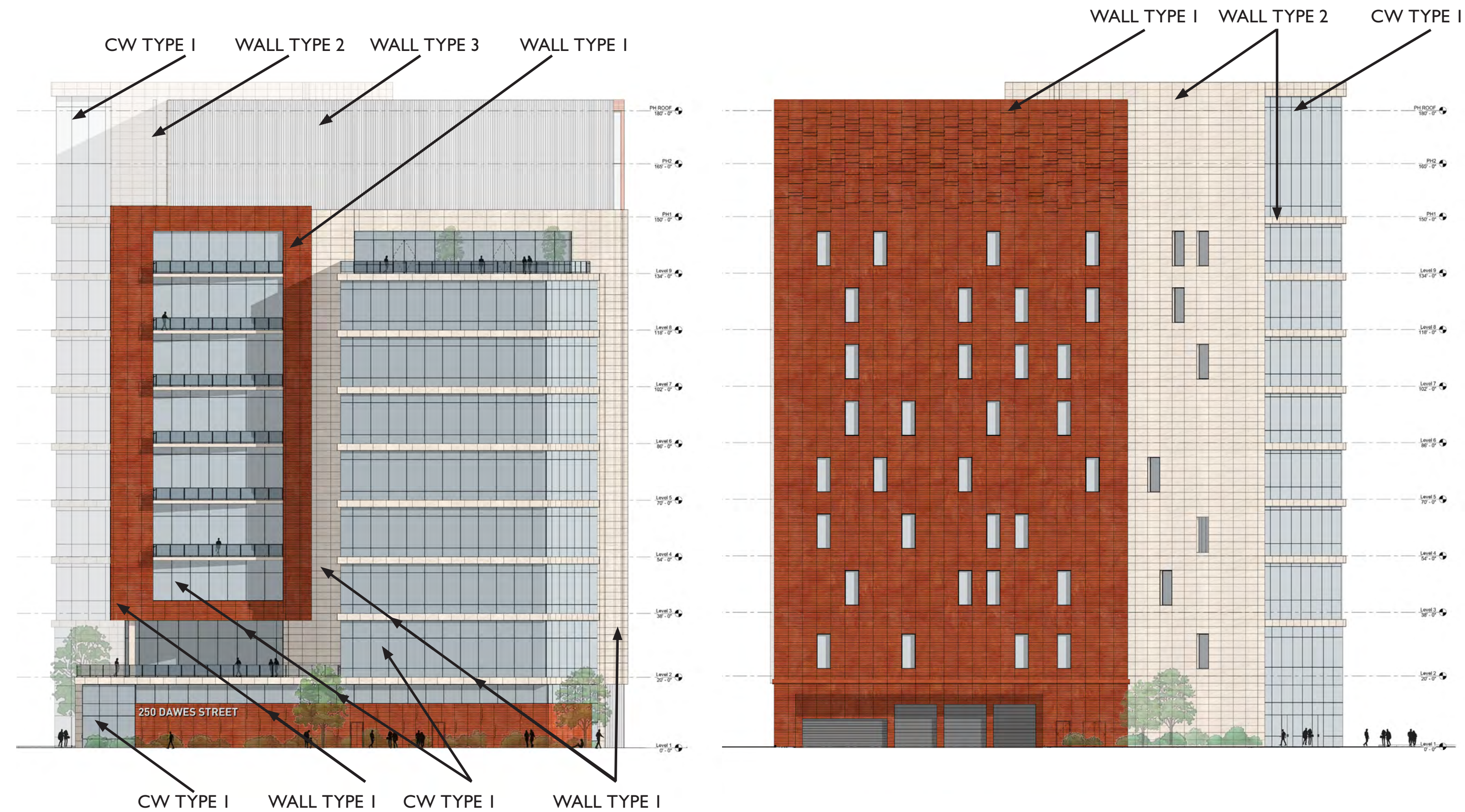
WALL TYPE I

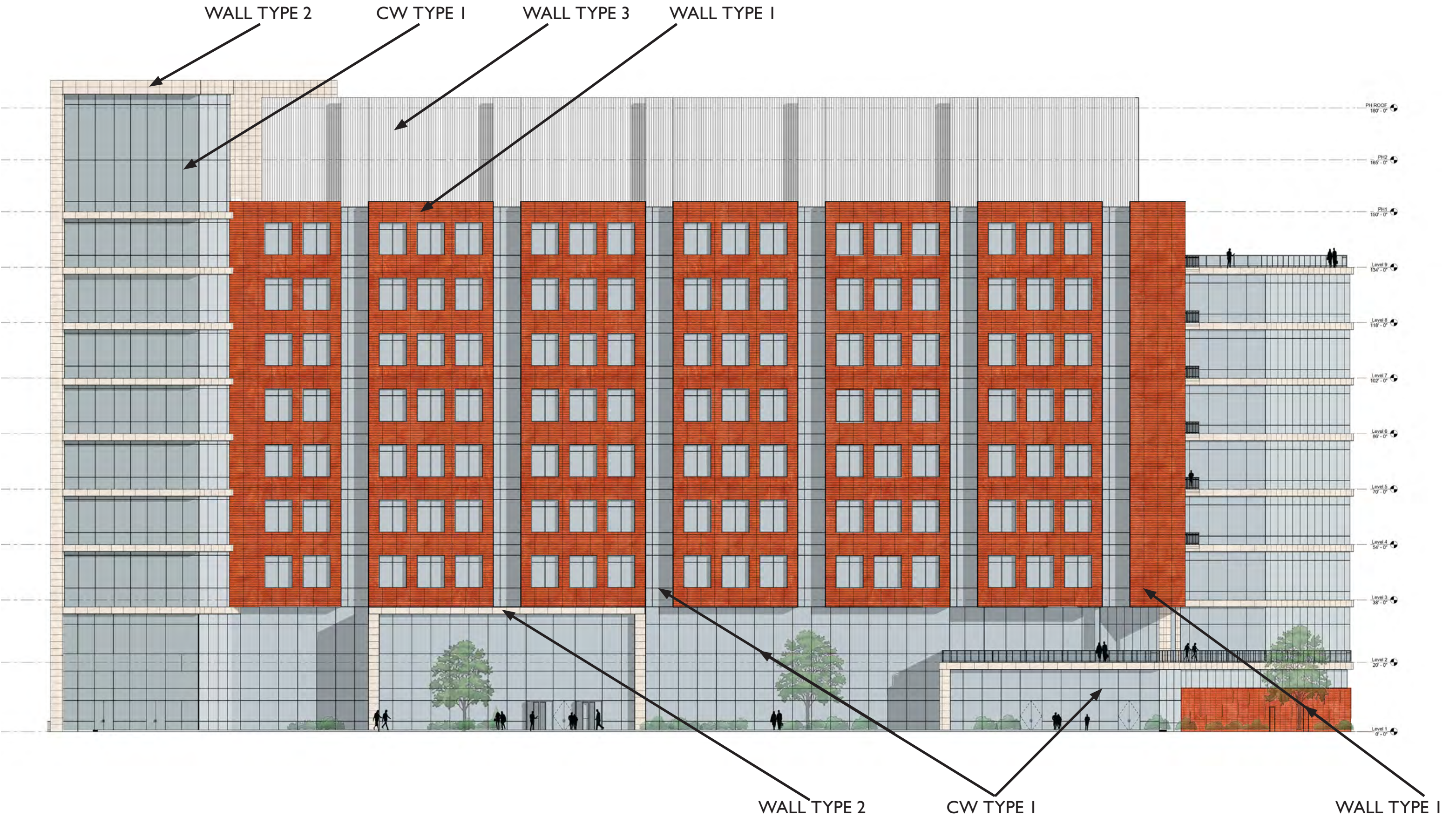
WALL TYPE 2

WALL TYPE 3

CW (CURTAIN WALL) TYPE I







PLAN DRAWINGS





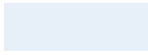


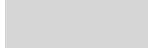
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0 5 10 20 40 ft.

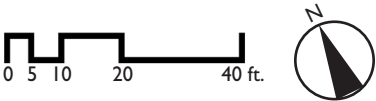
N

Ground Floor Plan

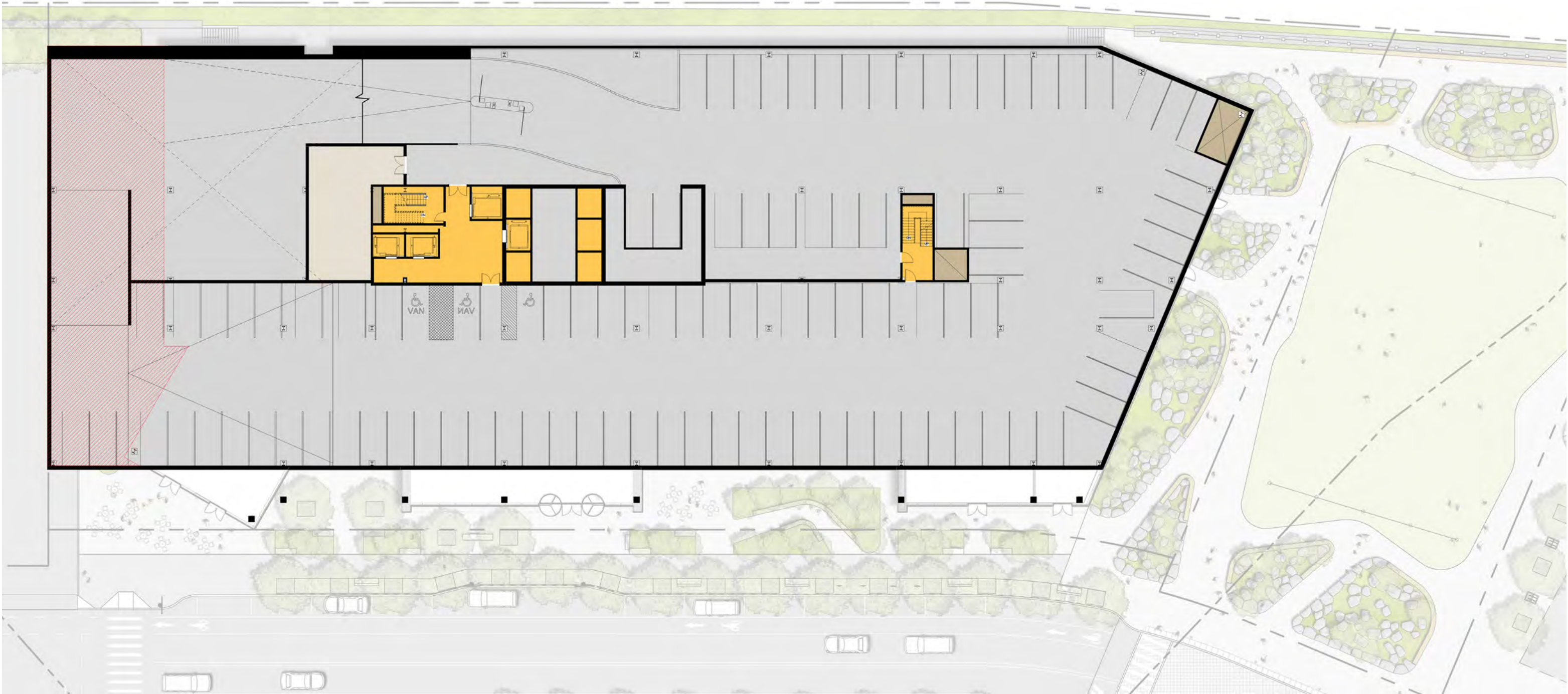


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|-------------------------------------------------------------------------------------------------------------------|------------------------------------------------------------------------------------------------------------|------------------------------------------------------------------------------------------------------------|
|  Secure Lobby |  Outdoor Amenity Area |  Building Shaft |
|  Mechanical / Building Support |  Tenant Space |  Vertical Circulation |
|  Active Use |  Parking | |

Plan Scale: 1/32" = 1' - 0"

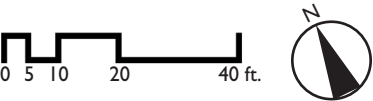


Parking Level I Plan



- | | | |
|-------------------------------------------|----------------------------------|----------------------------------|
| <div></div> Secure Lobby | <div></div> Outdoor Amenity Area | <div></div> Building Shaft |
| <div></div> Mechanical / Building Support | <div></div> Tenant Space | <div></div> Vertical Circulation |
| <div></div> Active Use | <div></div> Parking | |

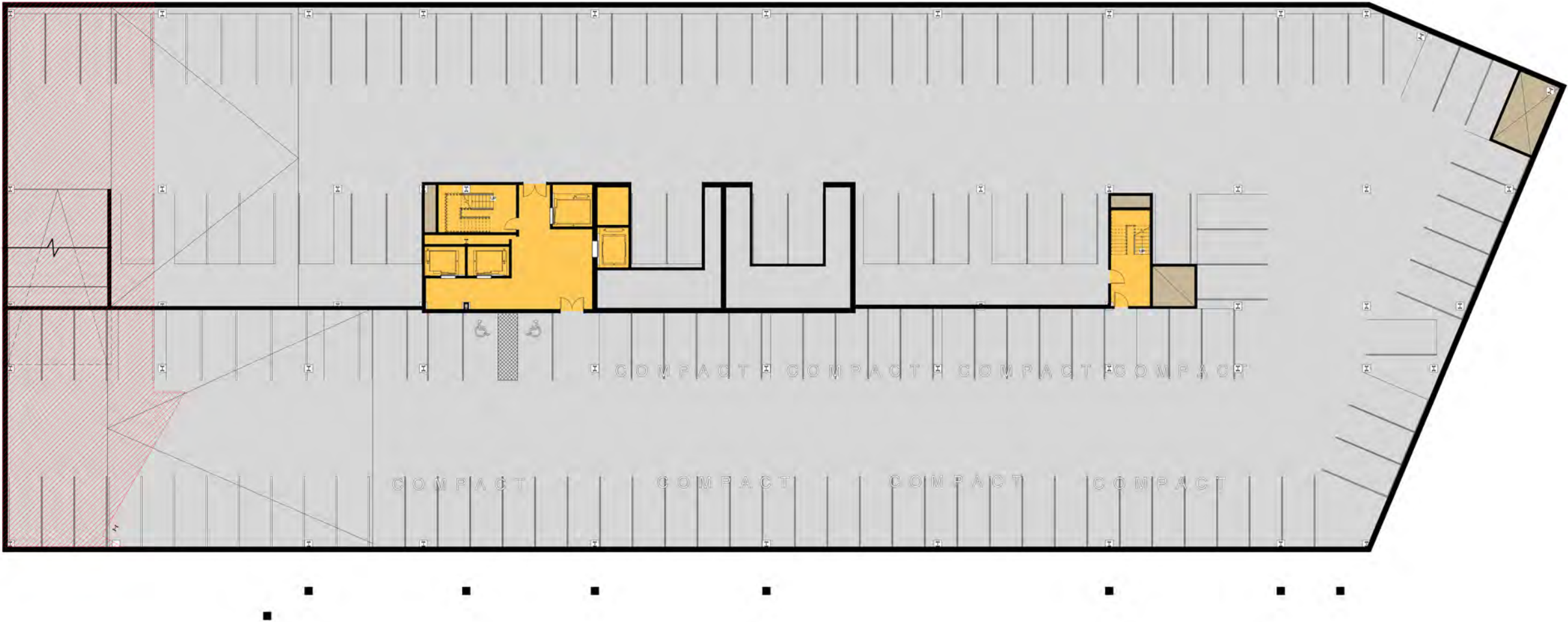
Plan Scale: 1/32" = 1' - 0"



This is a detailed architectural floor plan of a building. The central part of the plan features a large, irregularly shaped yellow-colored area, likely representing a common area or a specific functional zone. This yellow area contains several smaller rooms, including what appear to be offices or meeting rooms, and a central staircase. To the right of this central area, there is another yellow-colored section, also containing a staircase. The building's footprint is outlined in black, and the surrounding area is filled with a light gray grid pattern, representing a parking lot or an outdoor area. The plan includes various architectural details such as doors, windows, and furniture. A red hatched area is visible on the left side of the plan, possibly indicating a specific material or a boundary. The overall layout suggests a functional, multi-room building with a central common area and a large parking lot.

The diagram shows a path starting at a point, going up, then right 5 ft., then up, then right 10 ft., then up, then right 20 ft., then up, then right 40 ft., and finally up to end at a point. A compass rose indicates North is towards the top-left.

Parking Level 3 Plan

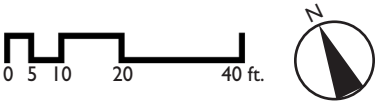


- Secure Lobby
- Mechanical / Building Support
- Active Use

- Outdoor Amenity Area
- Tenant Space
- Parking





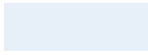


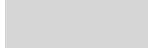
- Building Shaft
- Vertical Circulation

Plan Scale: 1/32" = 1' - 0"

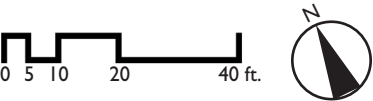


Ground Floor Plan







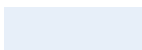



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|-------------------------------------------------------------------------------------------------------------------|------------------------------------------------------------------------------------------------------------|------------------------------------------------------------------------------------------------------------|
|  Secure Lobby |  Outdoor Amenity Area |  Building Shaft |
|  Mechanical / Building Support |  Tenant Space |  Vertical Circulation |
|  Active Use |  Parking | |

Plan Scale: 1/32" = 1' - 0"

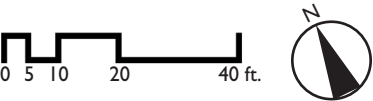


2ND FLOOR PLAN



- | | | |
|-------------------------------------------------------------------------------------------------------------------|------------------------------------------------------------------------------------------------------------|------------------------------------------------------------------------------------------------------------|
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|  Mechanical / Building Support |  Tenant Space |  Vertical Circulation |
|  Active Use |  Parking | |

Plan Scale: 1/32" = 1' - 0"



TYPICAL FLOOR PLAN - LEVEL 3

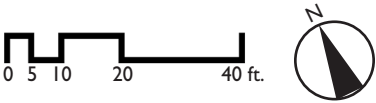


- Secure Lobby
- Mechanical / Building Support
- Active Use

- Outdoor Amenity Area
- Tenant Space
- Parking

- Building Shaft
- Vertical Circulation

Plan Scale: 1/32" = 1' - 0"



TYPICAL FLOOR PLAN - LEVELS 4-8

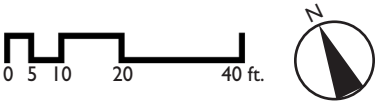


- Secure Lobby
- Mechanical / Building Support
- Active Use

- Outdoor Amenity Area
- Tenant Space
- Parking

- Building Shaft
- Vertical Circulation

Plan Scale: 1/32" = 1' - 0"



9TH FLOOR PLAN

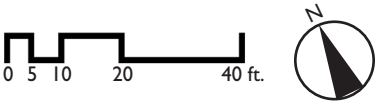


- Secure Lobby
- Mechanical / Building Support
- Active Use

- Outdoor Amenity Area
- Tenant Space
- Parking

- Building Shaft
- Vertical Circulation

Plan Scale: 1/32" = 1' - 0"

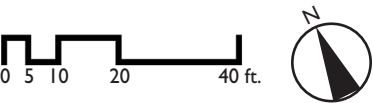


PHI FLOOR PLAN

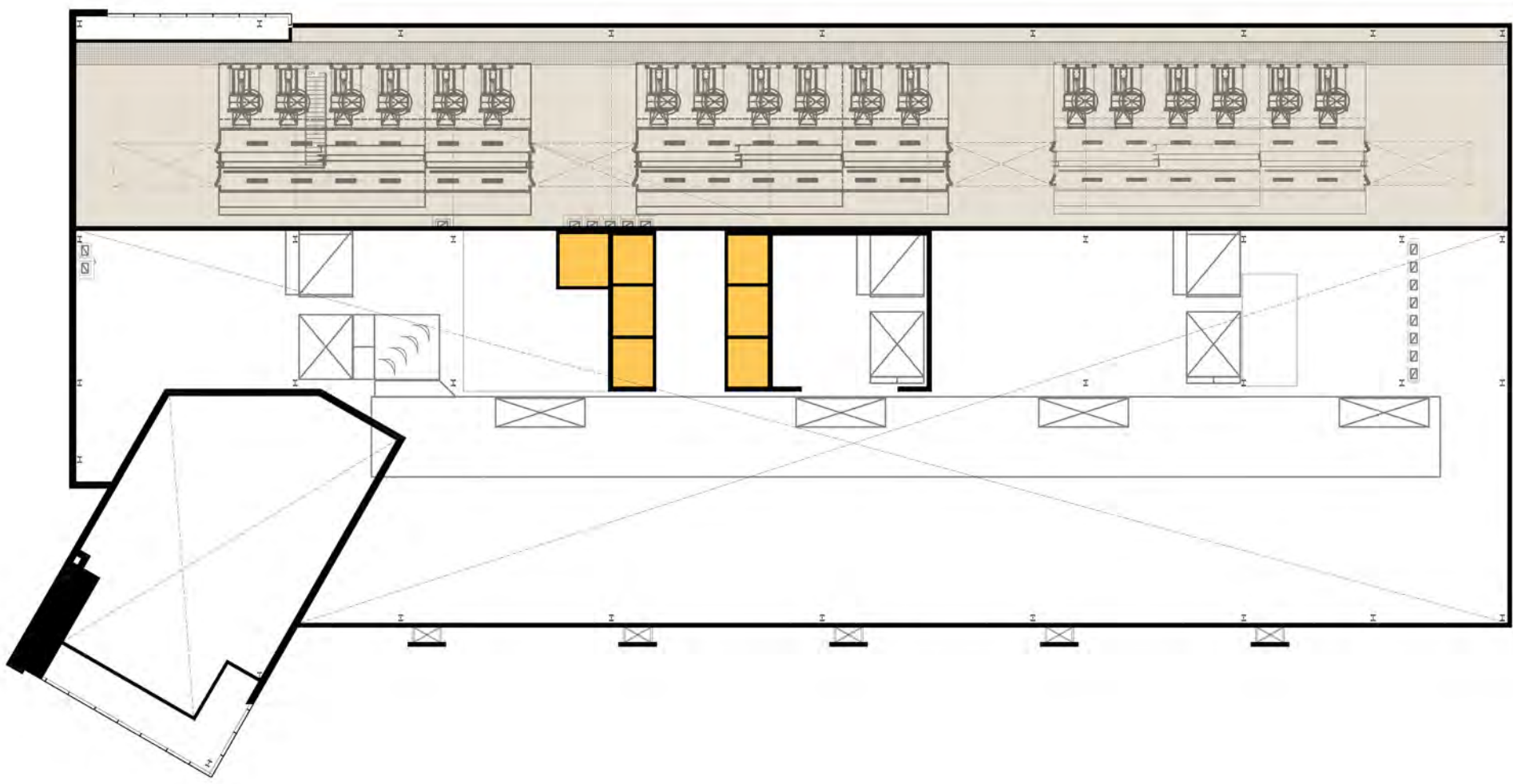


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|-------------------------------------------|----------------------------------|----------------------------------|
| <div></div> Secure Lobby | <div></div> Outdoor Amenity Area | <div></div> Building Shaft |
| <div></div> Mechanical / Building Support | <div></div> Tenant Space | <div></div> Vertical Circulation |
| <div></div> Active Use | <div></div> Parking | |


Plan Scale: 1/32" = 1' - 0"





PH2 FLOOR PLAN



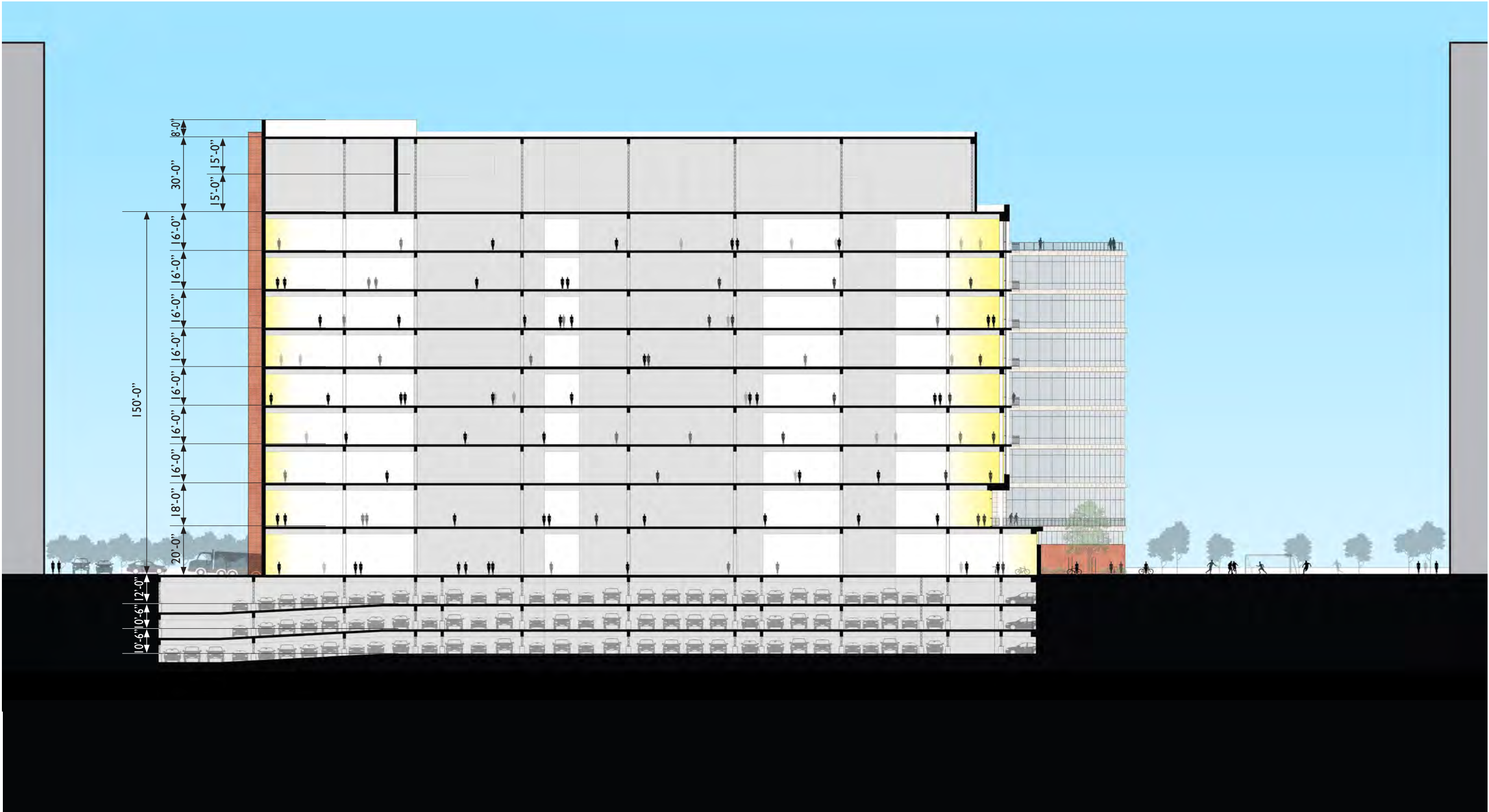
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| <div></div> Mechanical / Building Support | <div></div> Tenant Space | <div></div> Vertical Circulation |
| <div></div> Active Use | <div></div> Parking | |



Plan Scale: 1/32" = 1' - 0"



BUILDING SECTION

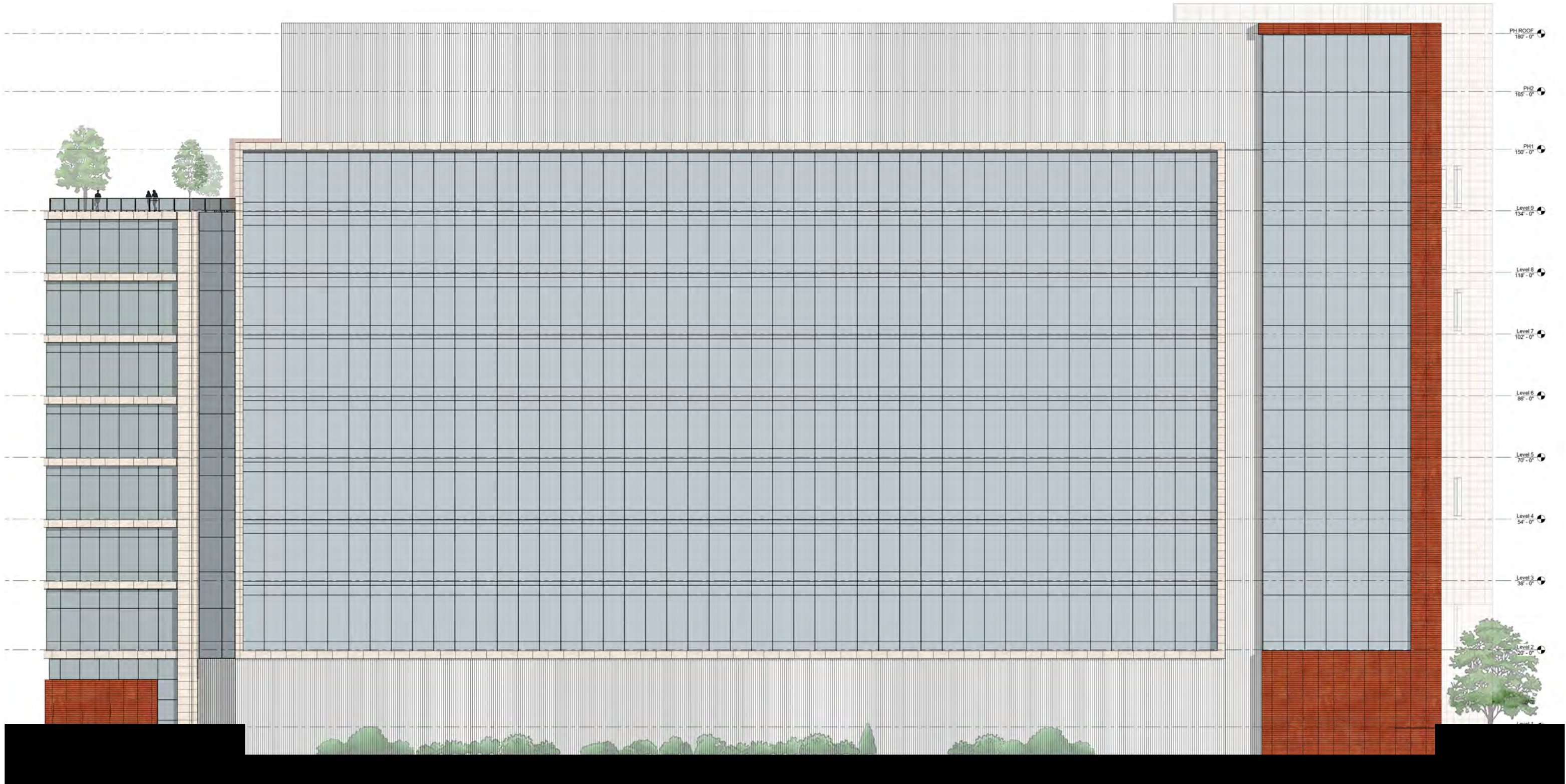


BUILDING ELEVATIONS

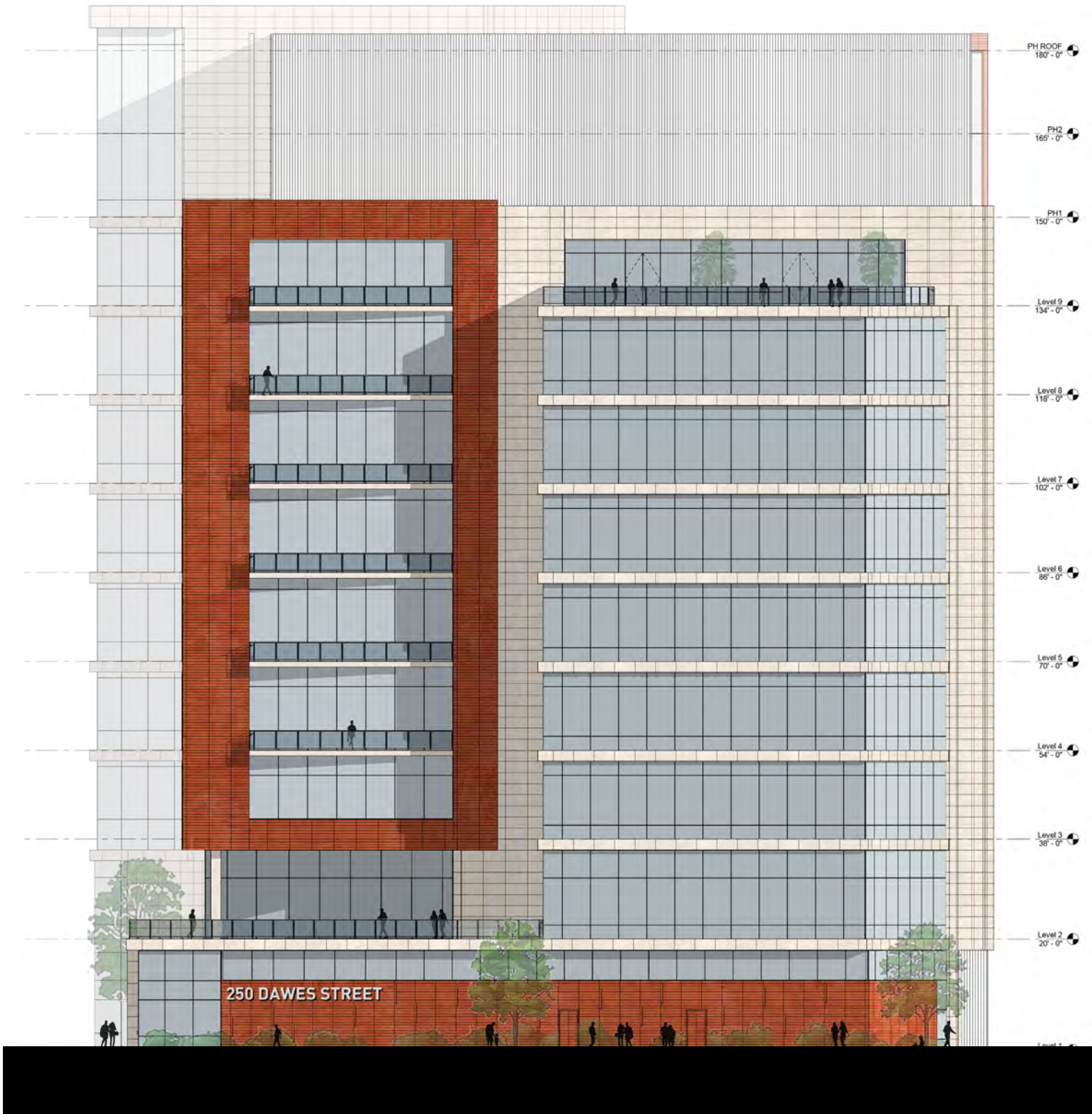
SOUTH ELEVATION



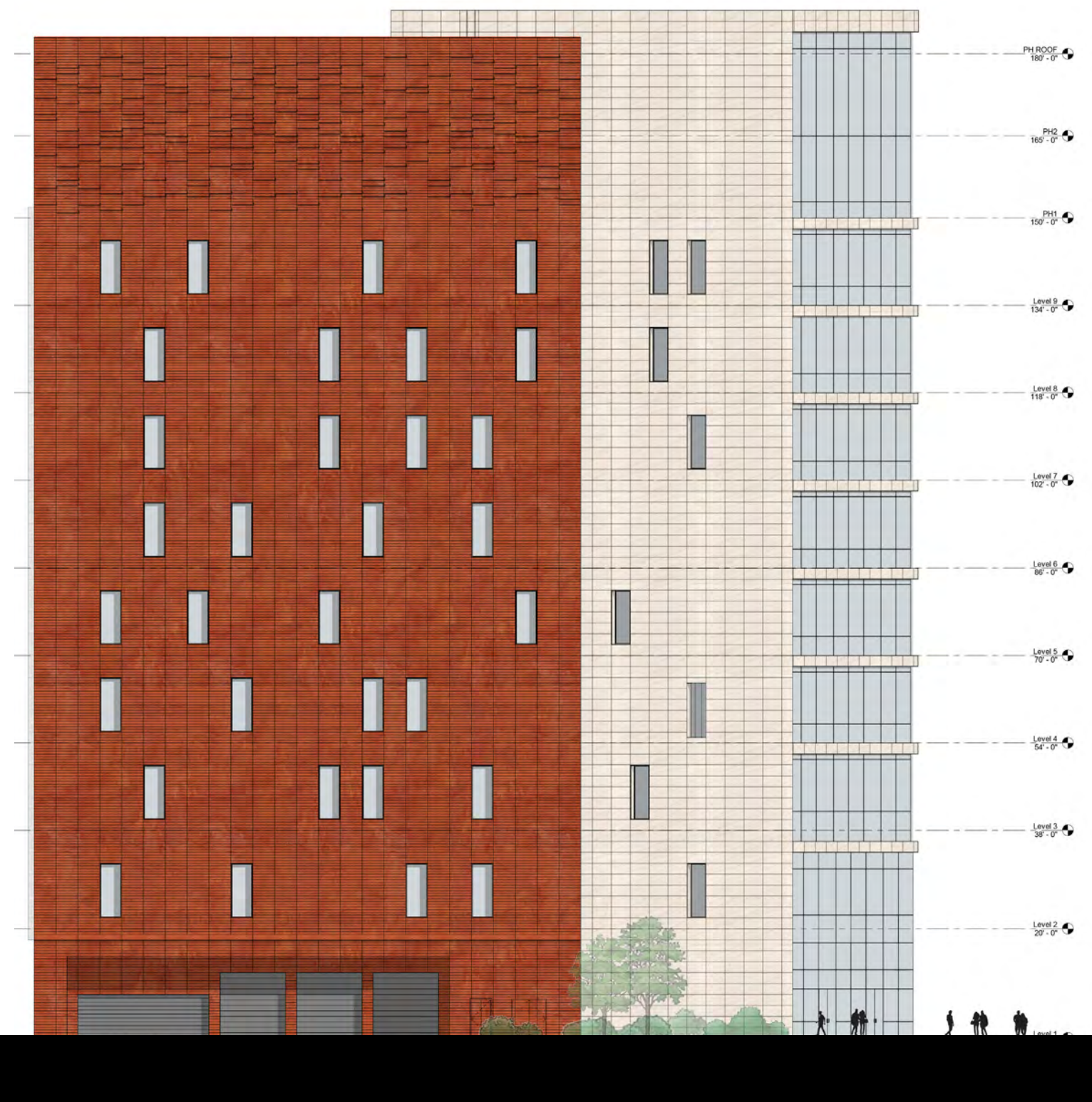
NORTH ELEVATION



EAST ELEVATION



WEST ELEVATION



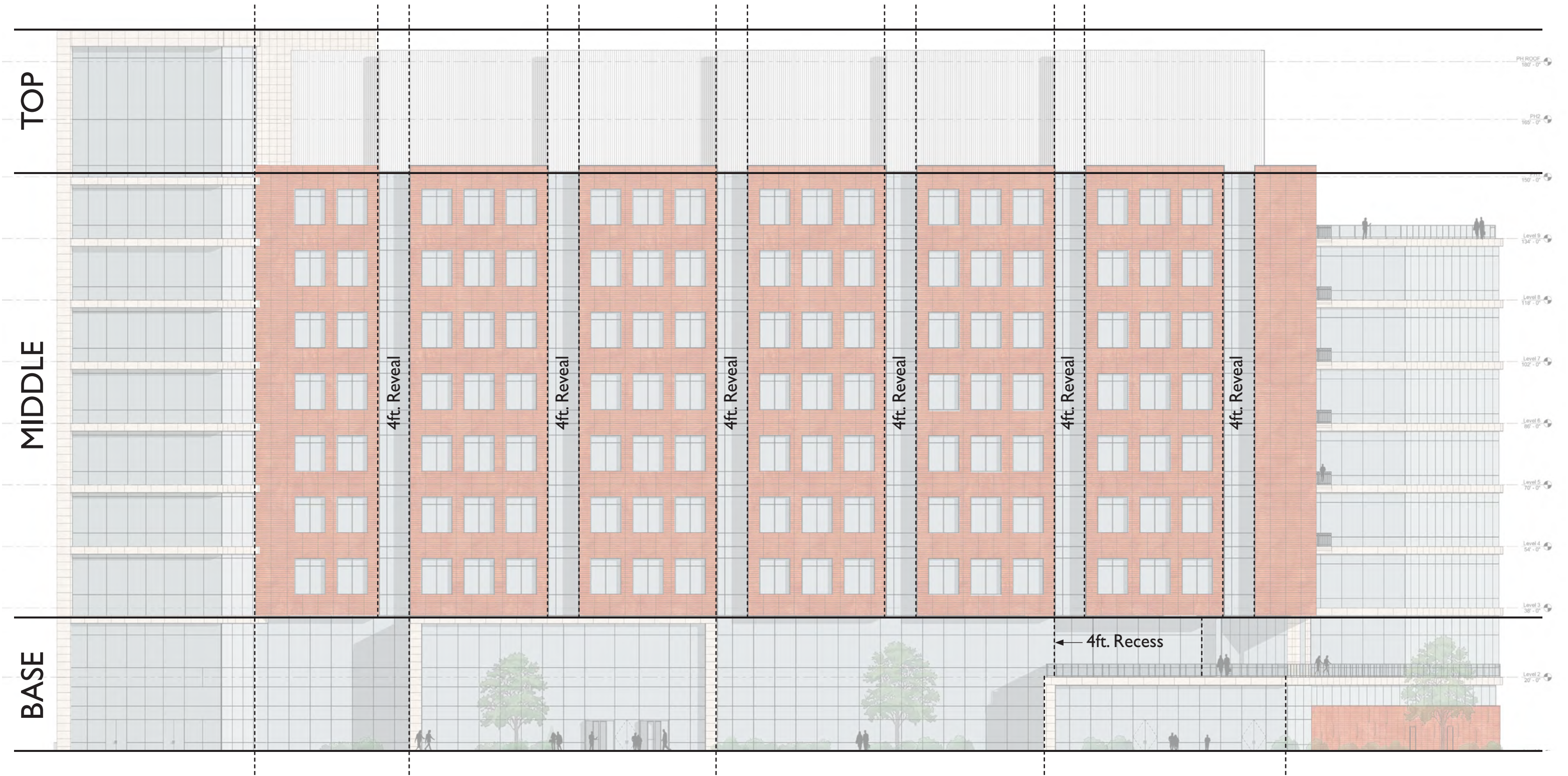
Solid to Void Analysis

Required Fenestration at Ground Floor Primary = 65% min.
Designed Fenestration at Ground Floor Primary = 69%

Required Fenestration at Upper Floors = 30% min - 50% max
Designed Fenestration at Level 2 = 49%
Designed Fenestration at Levels 3-9 = 42%



Primary Facade Vertical and Horizontal Articulation



1st Floor East Facade

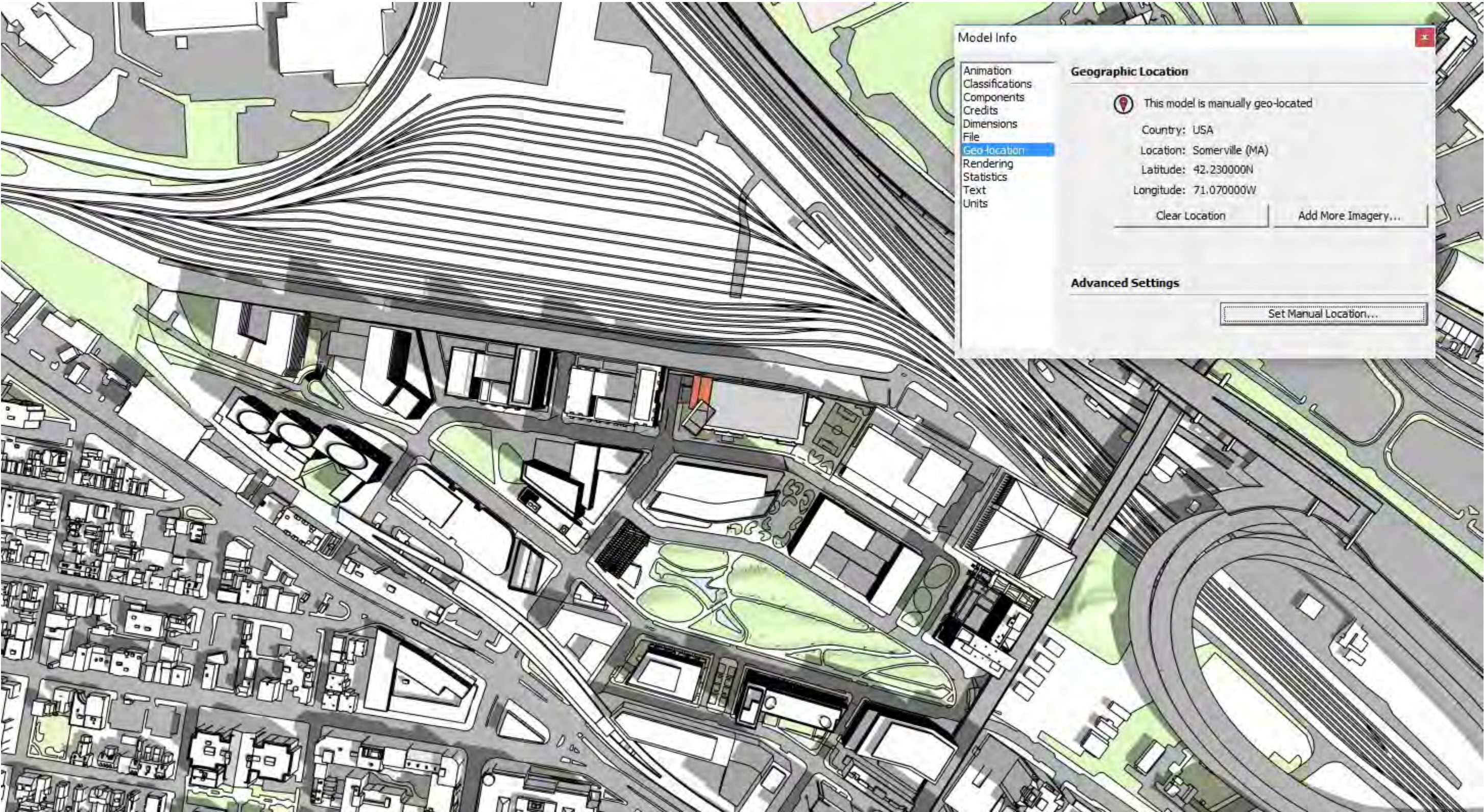


12" projected vertical Terracotta Panel



ENVIRONMENTAL ANALYSIS - SHADOW & GLARE STUDIES

Geo-location of Computer Generated Model



Existing and Net New Shadows - Parcels EF and G

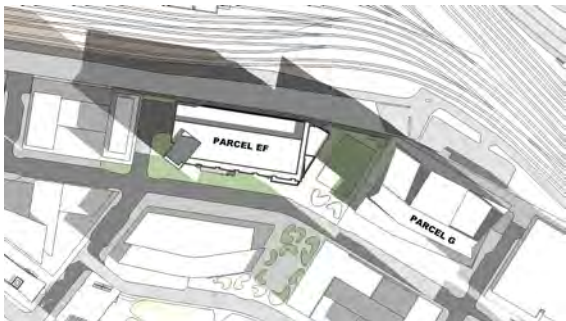
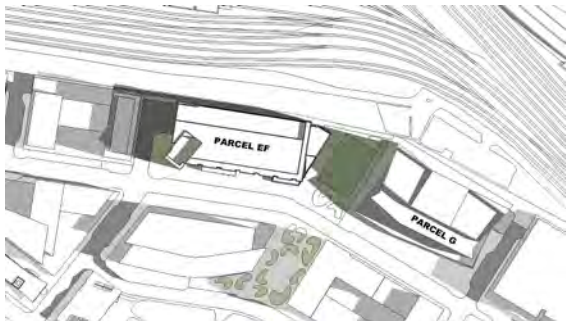
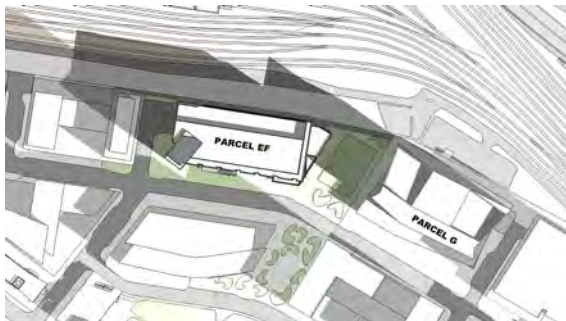
Vernal Equinox - MARCH 21

Summer Solstice - JUNE 21

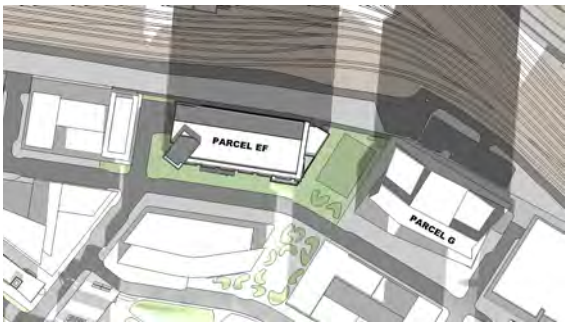
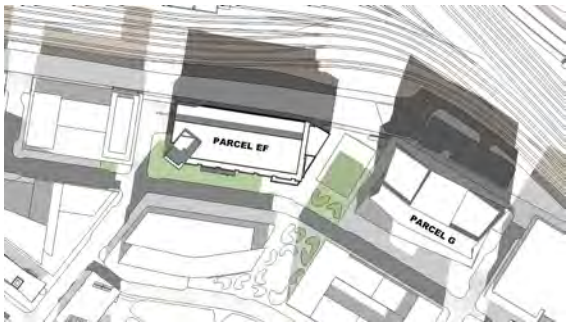
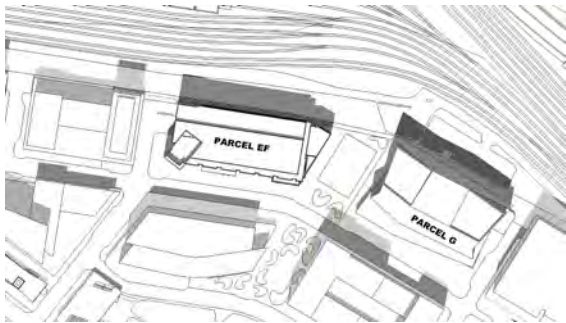
Autumnal Equinox - SEPTEMBER 21

Winter Solstice - DECEMBER 21

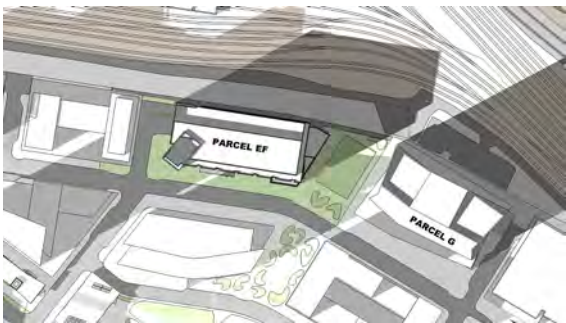
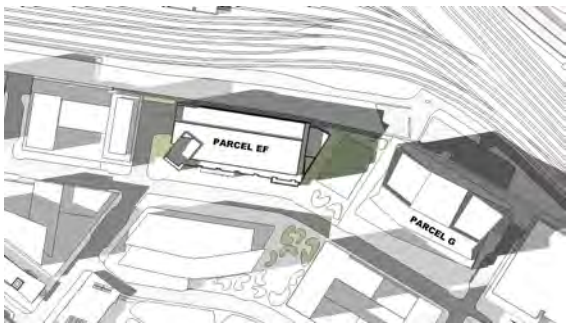
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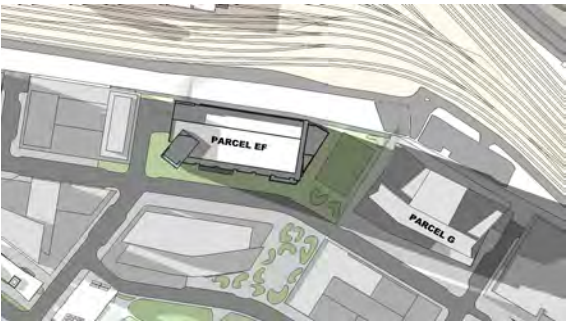
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3:00 PM



6:00PM

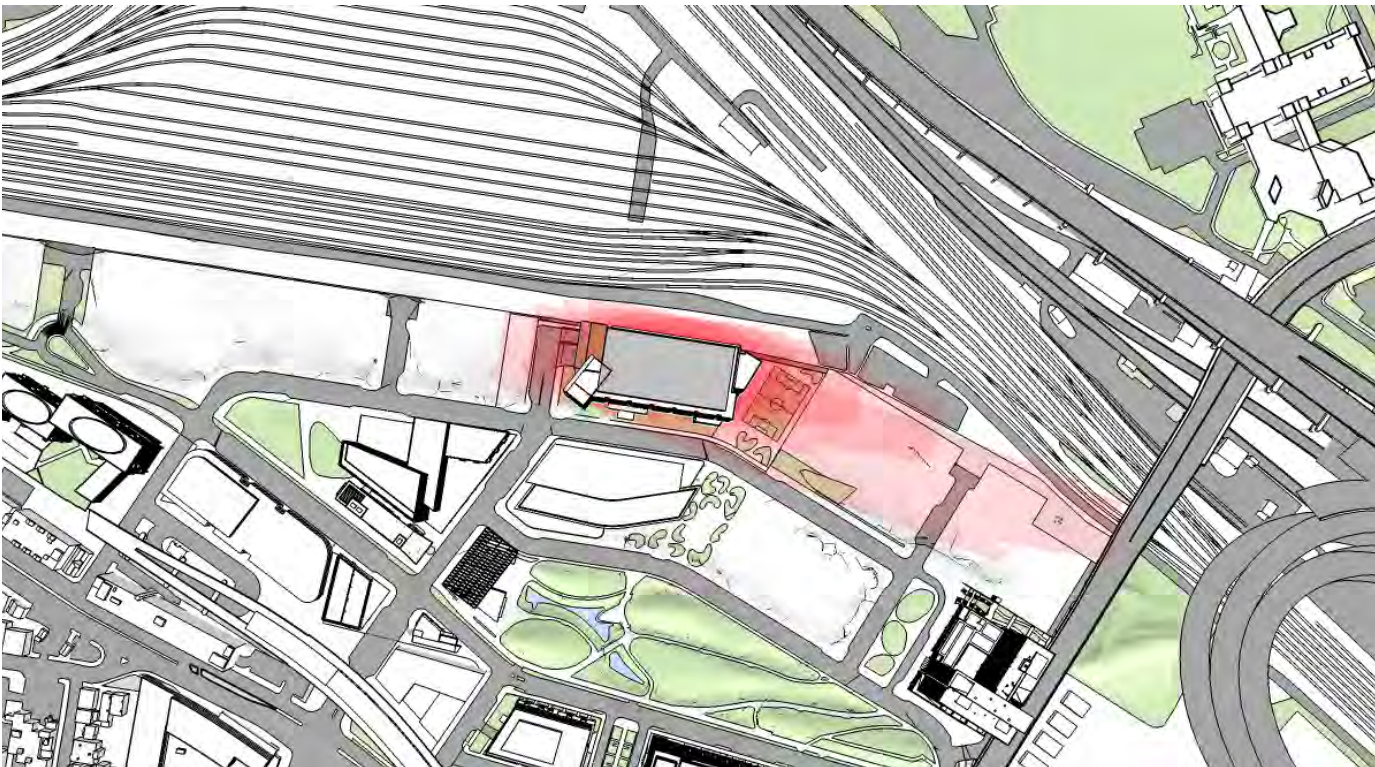


Cumulative New Shadows

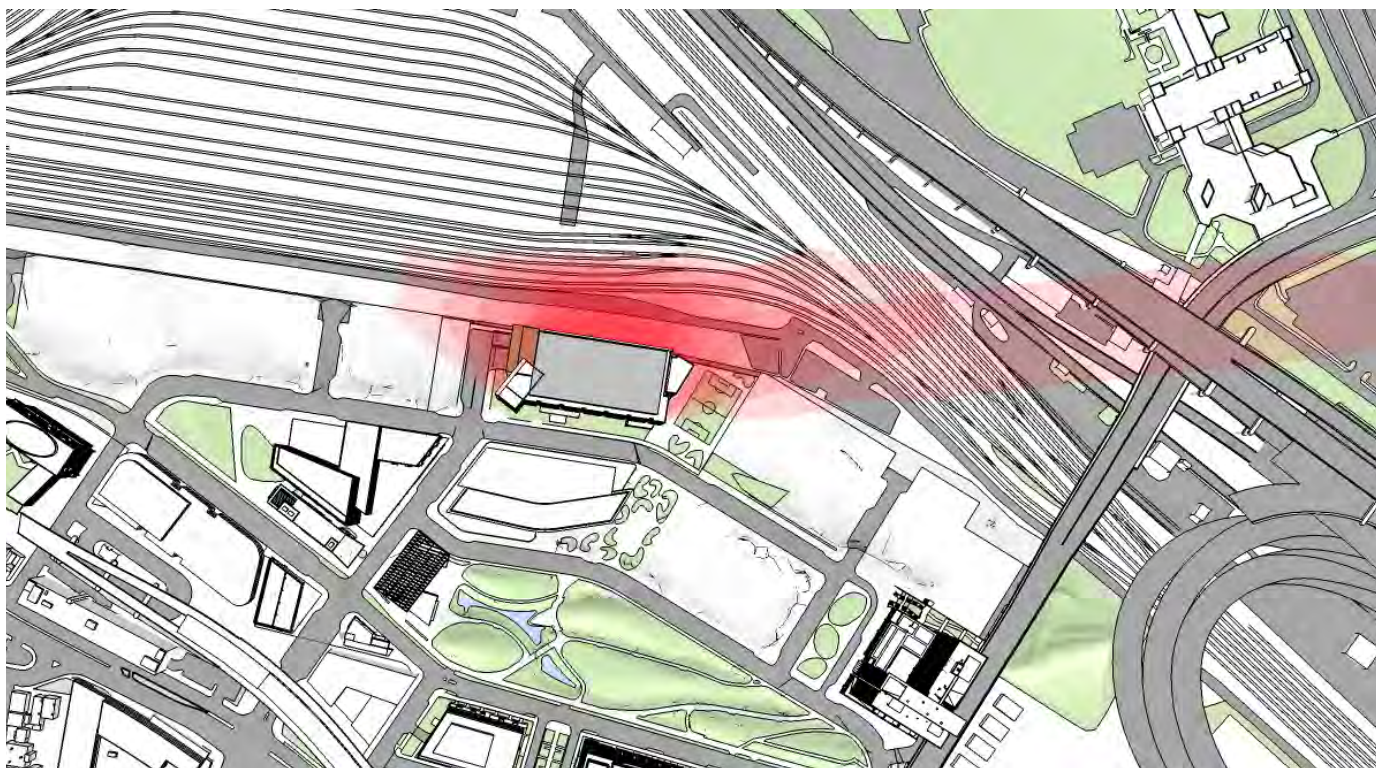
Vernal Equinox - MARCH 21



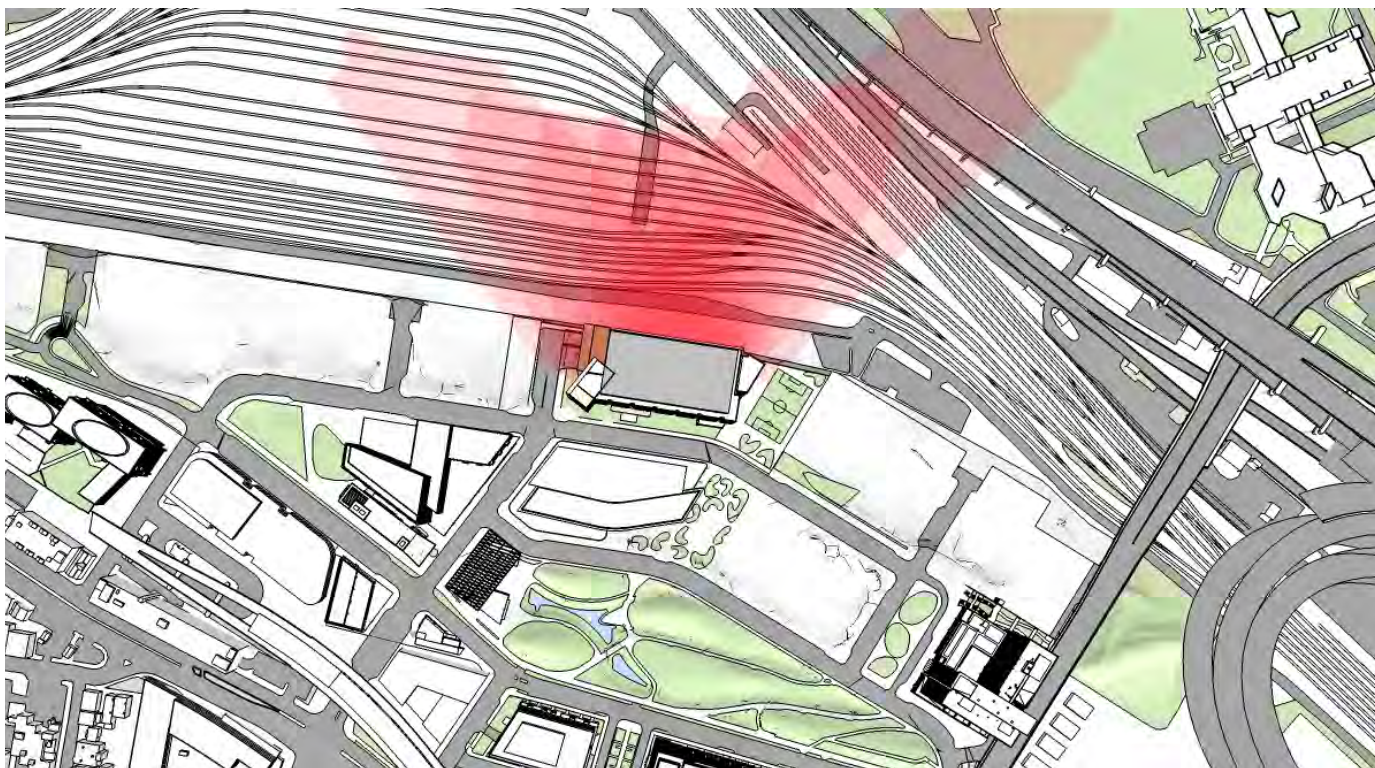
Summer Solstice - JUNE 21

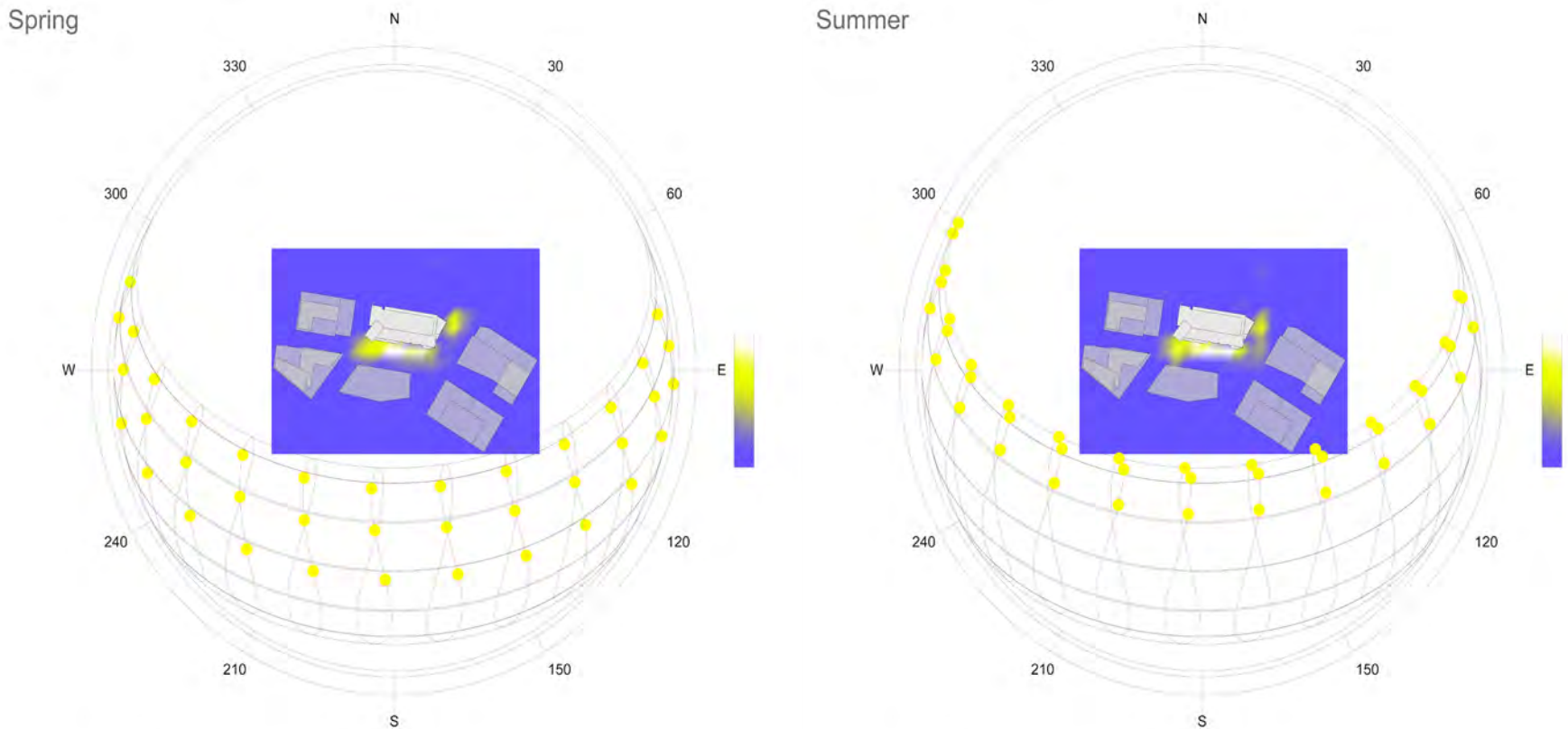


Autumnal Equinox - SEPTEMBER 21



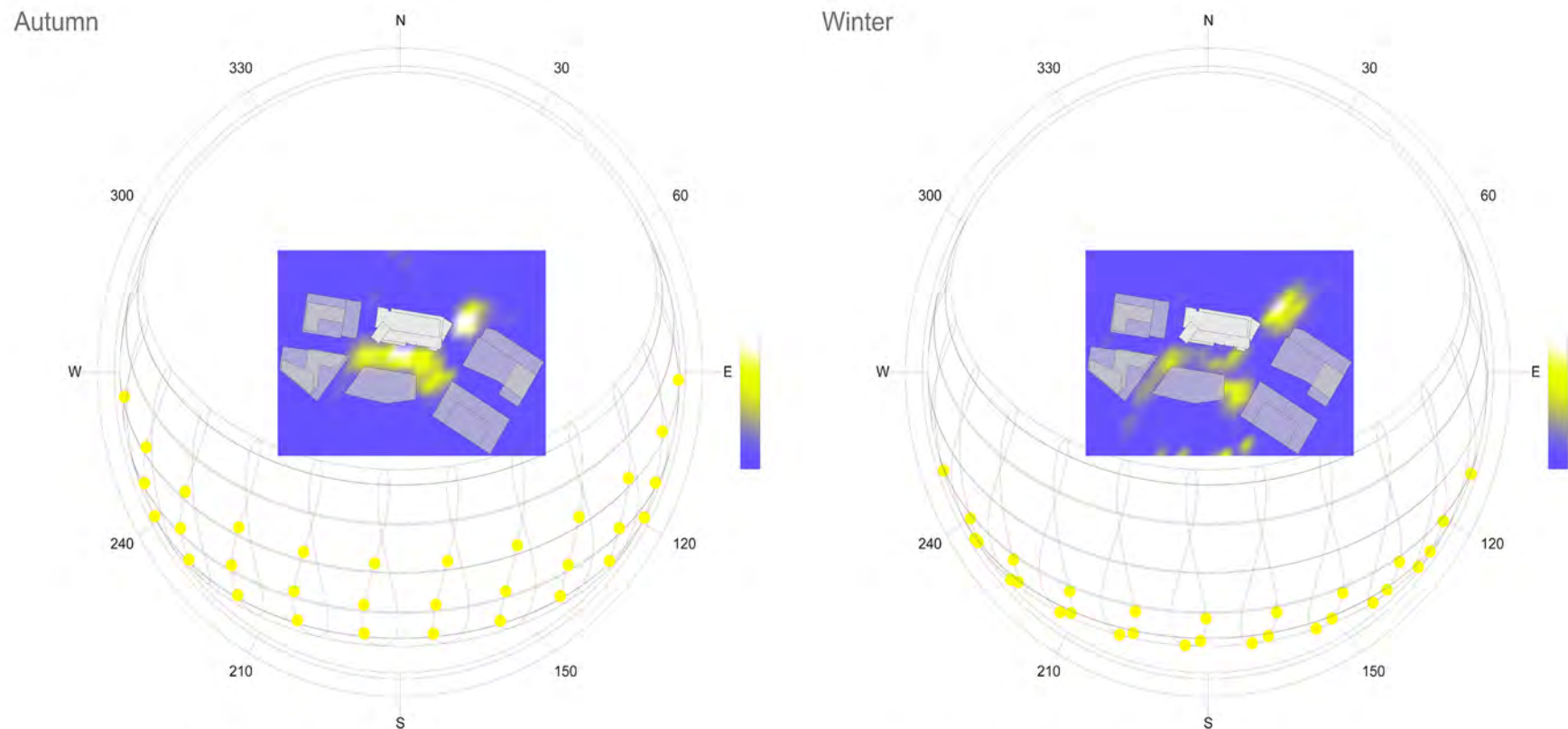
Winter Solstice - DECEMBER 21





Sun-Path Diagram - Latitude: 42.35




Sun-Path Diagram - Latitude: 42.35

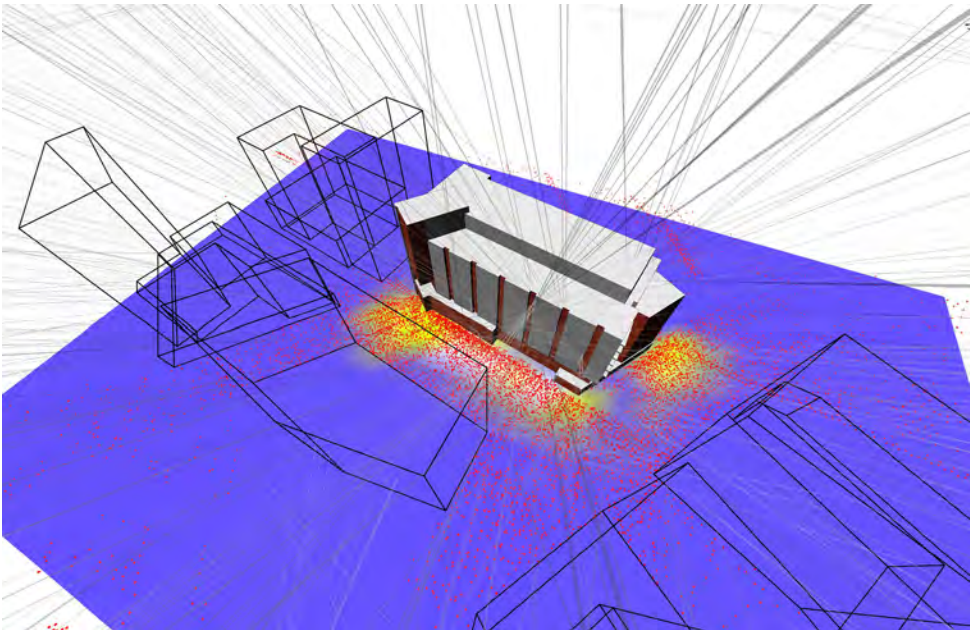


Sun-Path Diagram - Latitude: 42.35

Sun-Path Diagram - Latitude: 42.35

Point Distribution:

-  Negligible Accumulation
-  Moderate Accumulation
-  Greatest Accumulation



Annual Cumulative Point Distribution

The diagrams illustrate the distribution of light points across the seasons, as they are reflectedz from the glass in the proposed building design. The analysis does not indicate any adverse conditions over the course of the year.

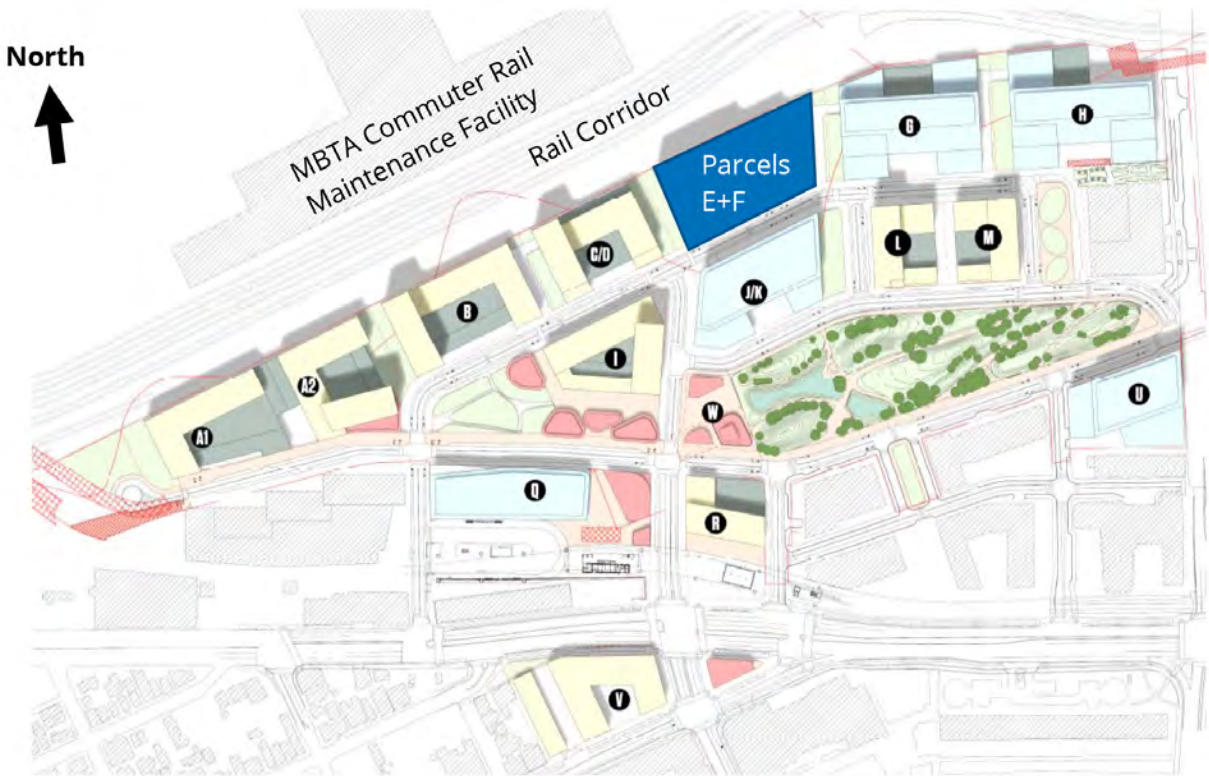


Image 1: Location of Parcels E+F of the Northpoint Development

Laboratory Exhausts

As the building will house research tenants, it is assumed that the exhaust air handling system will be connected to laboratory fume hoods. The total flow rate of exhaust air is anticipated to range from 168,000 cfm to 224,000 cfm. This volume will be connected to two or three large exhaust air handling units each with six fans. Review of the cut sheet for the proposed EAHUs indicate that the stacks will discharge 20 ft above the lower penthouse roof (5 ft above upper penthouse roof), and the discharge velocity will be at least 3,000 fpm. Based on this design, no significant concerns are anticipated at the proposed air intakes when the fans operate at full flow conditions. We are not aware of the location of air intakes on Parcels C+D, which is expected to be taller than E+F; it is possible that less dilution would be achieved at this building than we would recommend. If there are any air intakes on the east side of Parcels C+D, or there is a desire to operate the fans with reduced airflow volumes to save energy, we would recommend that wind tunnel testing be performed to quantify a safe lower limit to this flow. We recommend that the intakes on C+D be strategically placed to avoid impacts from Parcels E+F.

Diesel Generators

Two 1,500 kW diesel generators are located in the northeast corner of the penthouse. Having the exhaust stacks discharging vertically above the roof, without fixed rain caps, directly above the units would be positive as there would be good separation from the proposed air intakes. Since a large wake could form on the roof of the building during northerly winds, it will be important that detailed modelling be performed to identify an appropriate stack height to limit air quality impacts. Wind tunnel testing can also be used to predict the frequency of winds that would have the potential to cause odors at the air intake. In general, testing the generators during hours of low occupancy can be an effective strategy to reduce the potential for odor concerns.

Cooling Towers

The cooling towers are situated on the highest point of the building with the air intakes situated below, which is positive from a dispersion perspective. No significant air quality concerns are anticipated at Parcels E+F provided that low-odor/low-toxicity water treatment chemicals are used. This conclusion is predicated on not locating the intakes on Parcel C+D on the east façade, which could lead to serious impacts from the E+F towers. To prevent the spread of Legionella bacteria, we recommend that the towers be maintained according to manufacturer recommendations and practices outlined by ASHRAE¹ and the Cooling Tower Institute².

Boilers

Three to six boilers are anticipated for the penthouse. Discharging the flues vertically above the roof without fixed rain caps will result in higher levels of dispersion as compared to other flue configurations (e.g., discharging horizontally, or from a vertical flue with a rain cap). Provided that the flue discharges vertically, are separated from the air intake by at least 50 ft and the units are low-NO_x natural gas boilers, no significant concerns due to pollutant levels would be anticipated.

Rail Corridor

Parcels E+F back onto a rail corridor where it is assumed that diesel locomotives travel. A Massachusetts Bay Transportation Authority (MBTA) Commuter Rail Maintenance Facility is located just to the north of this site. The MBTA operates a fleet of diesel locomotives and utilizes this rail corridor for regular commuter rail service. Therefore, it is assumed that there will be regular and frequent diesel locomotive traffic to the north of Parcels E+F.

The air intakes on the south façade of the proposed building are well situated to increase separation distance and prevent “line-of-sight” between the intakes and locomotives. This protection will improve the level of dilution between the engine exhaust before reaching the air intakes. While the engine exhaust is expected to be sufficiently diluted such that no significant health-related concerns are expected, it is possible that detectable levels of odor could reach the fresh air intakes during northerly winds. It is recommended that wind tunnel modeling be performed to quantify the risk for odors from locomotives and determine whether the air intakes should be treated using activated carbon filters to prevent nuisance odors from entering the building.

Sources on Parcels J+K

The highest architectural elevation of the Parcel E+F building is 188 ft above grade. Based on our previous work on Parcels J+K, we understand that that building will be of a similar height, and will feature laboratory exhausts and a diesel generator discharging above roof level. The Parcel J+K laboratory exhausts are not expected to present a significant concern at Parcels E+F owing to the high flow rate which offers significant levels of dilution.

The Parcel J+K generator is not a concern for the proposed air intakes due to pollutant levels; detectable levels of odors could enter Parcel E+F when the generator operates during southerly winds. Given the infrequency of generator operation (i.e., scheduled testing and during loss of power), this may not be a significant concern at Parcel E+F.

Sources On Other Parcels

We are not aware of the specific exhausts on any parcels other than J+K. The roof of the adjacent Parcels C+D is expected to be approximately 50 ft higher than that of Parcels E+F. As a result, the exhausts on the roof of this parcel are unlikely to be a significant concern at Parcels E+F. Parcel G is expected to be approximately 10 to 20 ft lower in height than Parcels E+F, however with the fresh air intakes on the south façade, sources on this roof are unlikely to be a significant concern.

ENVIRONMENTAL ANALYSIS - EXISTING SITE CONDITIONS

Existing Site Conditions



Existing Site Conditions - Looking North



Existing Site Conditions - Looking East



Existing Site Conditions - Looking South

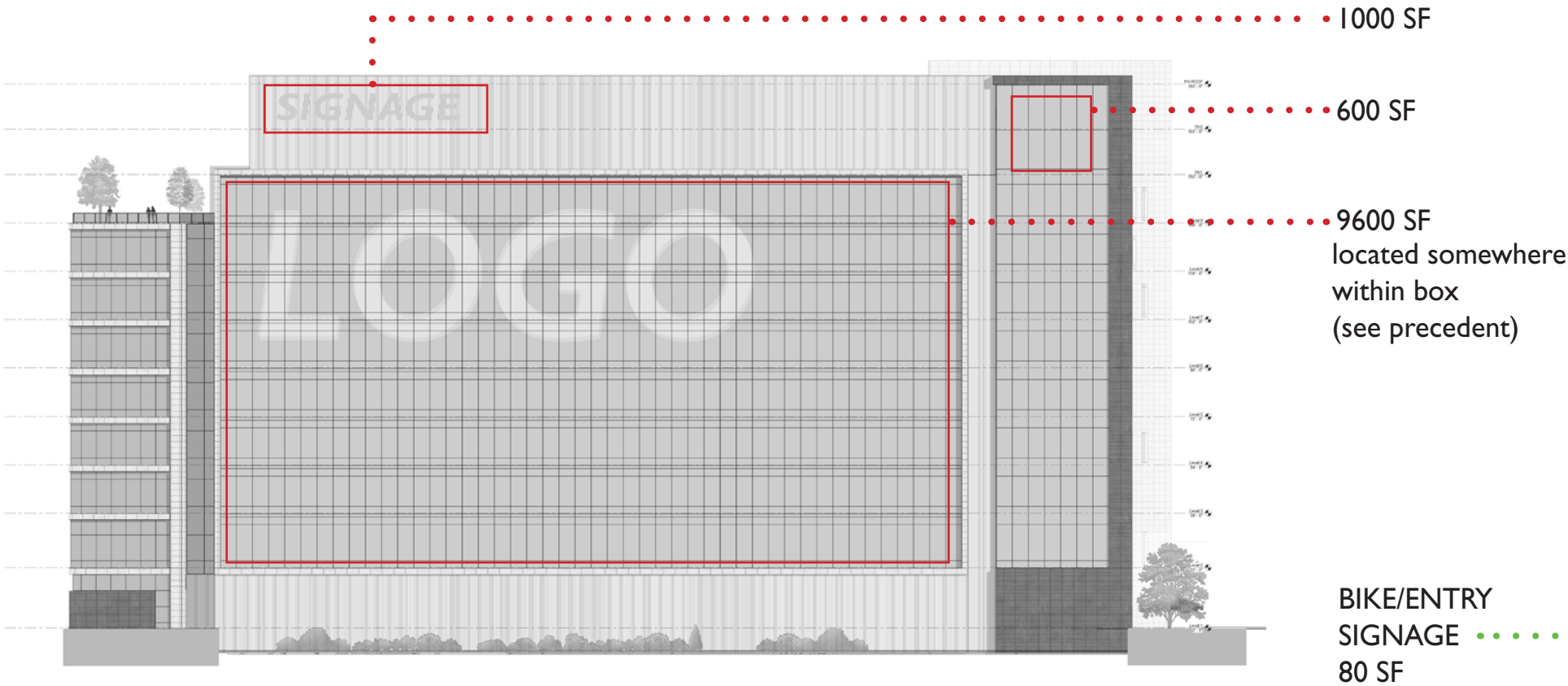


Existing Site Conditions - Looking West

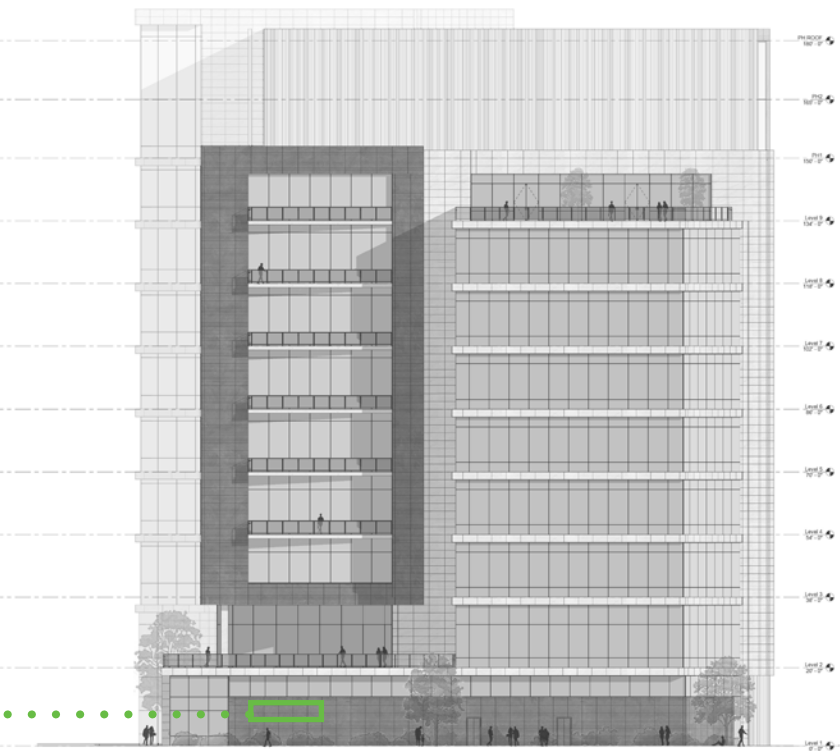


BUILDING SIGNAGE

North Elevation



East Elevation



South Elevation



12.4.4 Calculation of Sign Area.

d. In calculating the permitted sign area, the sign frontage shall be understood to mean the length of a building along a public way occupied by a separate and distinct use or that length of a building that is set back from, but facing a public way where such a sign would be highly visible. The total area in square feet of all permanent signs on a sign frontage, except for signs on windows above the first floor, free-standing signs, directional and public purpose signs shall not exceed from sign frontage multiplied by the appropriate factor.

EF Sign Frontage

building frontage X frontage factor (avg. distance of sign from street center line 0-99 feet)

320' x 2 = 640 sf allowable

 Allowable Subject to Confirmation

EF Sign Frontage : Somerville Allowable

building frontage X frontage factor (avg. distance of sign from street center line 0-99 feet)

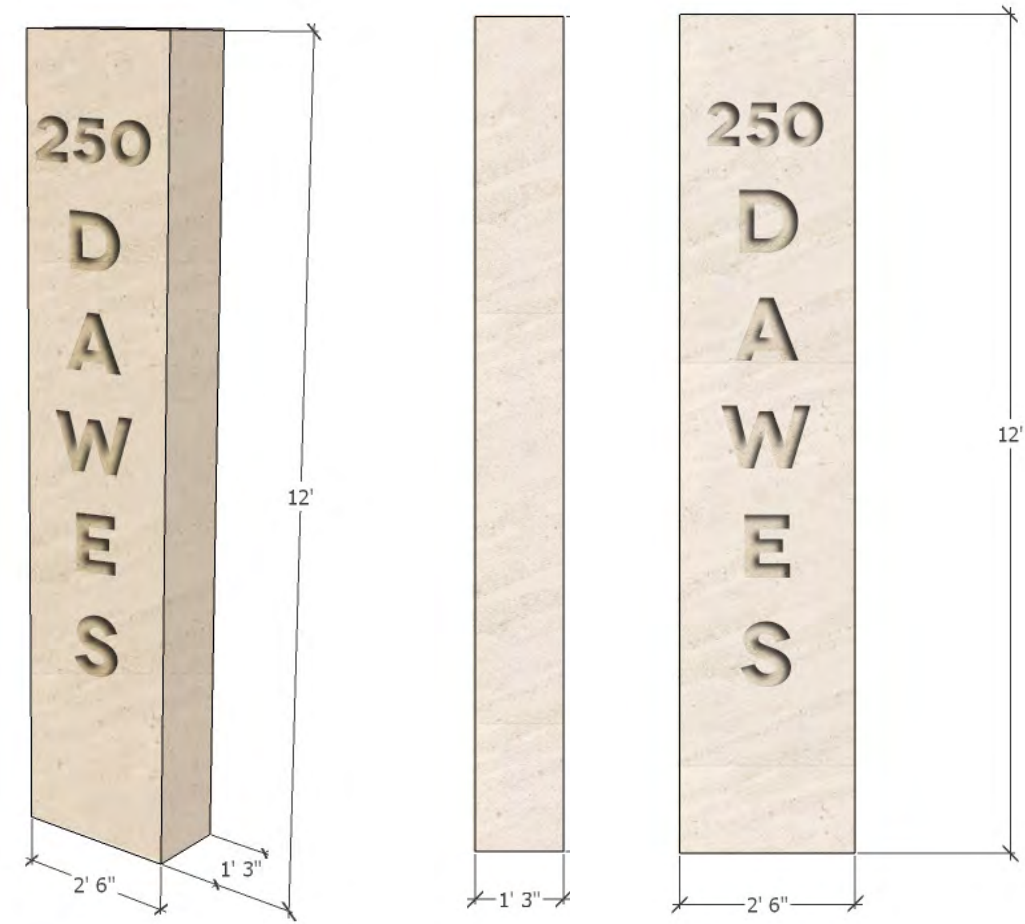
320' x 2 = 640 sf allowable

Somerville Signage Calculation

Retail Signage	40 sf
Awning Signage	40 sf
Entry Monument (Not Included in Total)	30 sf x 2 sides = 60 sf
Tenant/Bike Entry	80 sf
TOTAL Proposed Somerville Signage (at Ground Floor)	160 sf
Penthouse North	1000 sf
First St.Tower	300 sf
Railyard Tower	600 sf
Supergraphic Area	9600 sf
TOTAL Somerville Signage (above Ground Floor)	11500 sf
TOTAL Proposed Somerville Signage at Frontage	460 sf
(First St.Tower + Ground Floor)	
TOTAL Proposed Somerville Signage	11960 sf

Monument Signage

Main Entry Elevation



North Elevation





Southwest Tower View



Southeast View



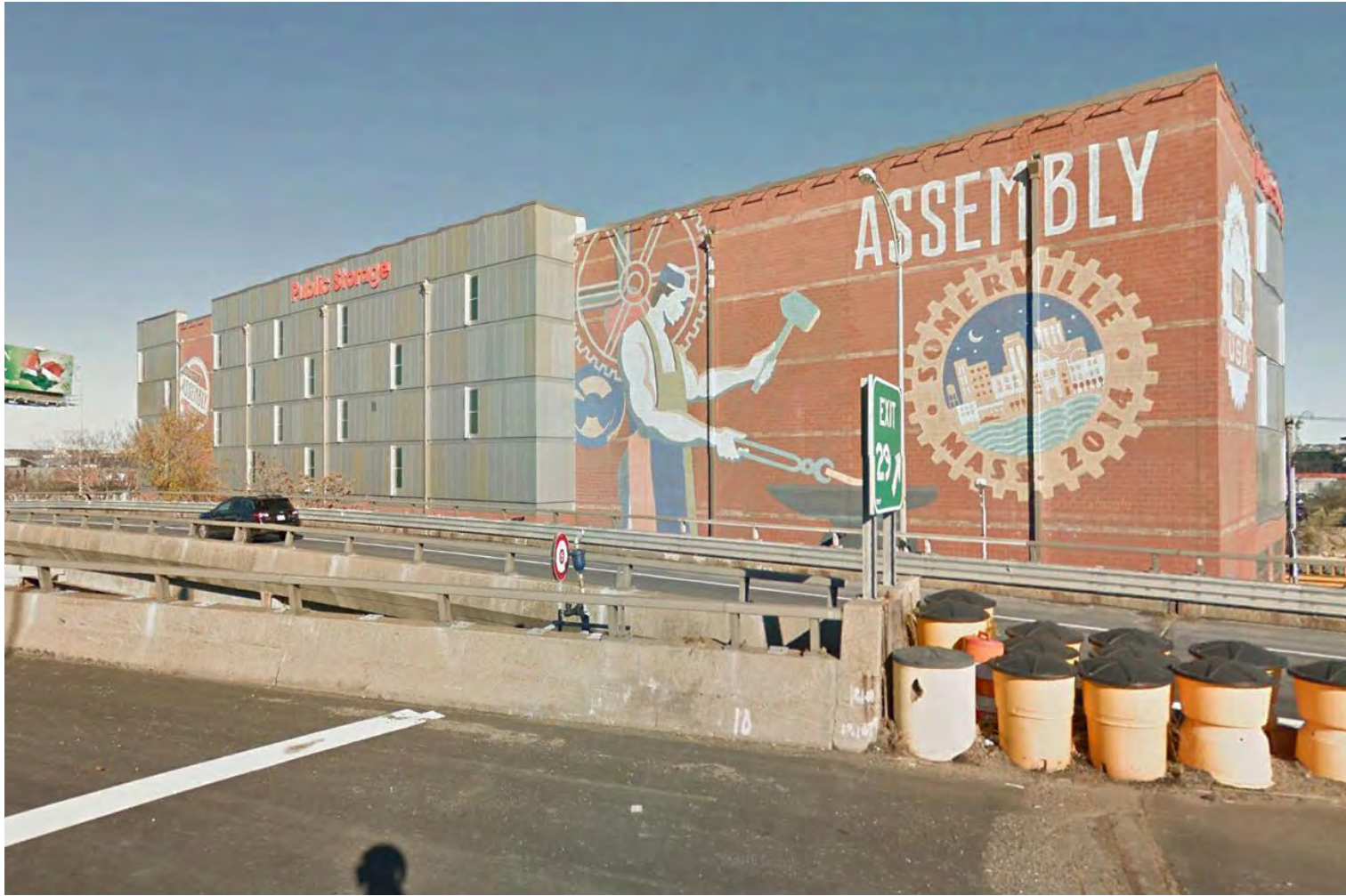
Northwest View



Supergraphic Precedent



Cambridge/Somerville Signage Precedent



GENERAL NOTES

THE CONTRACTOR SHALL MAKE ALL NECESSARY CONSTRUCTION NOTIFICATIONS AND APPLY FOR AND OBTAIN ALL NECESSARY CONSTRUCTION PERMITS. THE CONTRACTOR SHALL ALSO PAY ALL FEES AND POST ALL BONDS ASSOCIATED WITH THE SAME, AND COORDINATE WITH THE ENGINEER AND ARCHITECT AS REQUIRED.

CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR JOB SITE SAFETY AND ALL CONSTRUCTION MEANS AND METHODS.

LIMIT OF WORK SHALL BE EROSION CONTROL BARRIERS, LIMIT OF GRADING AND SITE PROPERTY LINES AND/OR AS INDICATED ON DRAWINGS.

PORTIONS OF THE ROADWAY, SIDEWALK AND ROADSIDE AREA DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED TO THEIR CONDITIONS PRIOR TO DISTURBANCE.

CONTRACTOR TO VERIFY UTILITY STUB LOCATIONS AND ELEVATIONS IN THE FIELD PRIOR TO COMMENCING WORK.

ANY ALTERATION TO THESE DRAWINGS MADE IN THE FIELD DURING CONSTRUCTION SHALL BE RECORDED BY THE CONTRACTOR ON RECORD DOCUMENTS.

ANY AREA OUTSIDE THE LIMIT OF WORK THAT IS DISTURBED SHALL BE RESTORED TO ITS ORIGINAL CONDITION AT NO COST TO OWNER.

EXISTING TREES AND SHRUBS OUTSIDE THE LIMITS OF GRADING SHALL BE REMOVED ONLY UPON PRIOR APPROVAL OF THE OWNER. FOR DRAWING LEGIBILITY, ALL EXISTING TOPOGRAPHIC FEATURES, EXISTING UTILITIES, PROPERTY BOUNDARIES, EASEMENTS, ETC. MAY NOT BE SHOWN ON ALL DRAWINGS. REFER TO ALL REFERENCED DRAWINGS AND OTHER DRAWINGS IN THIS SET FOR ADDITIONAL INFORMATION.

ALL EXCAVATORS OR CONTRACTORS MUST REFER TO 520 CMR 14.00 TO OBTAIN A TRENCH PERMIT PRIOR TO ANY CONSTRUCTION RELATED TRENCHES ON SITE.

EROSION CONTROL AND SEDIMENTATION NOTES

CONTRACTOR SHALL MAINTAIN ALL EROSION CONTROL MEASURES DURING ENTIRE CONSTRUCTION PERIOD.

ANY SEDIMENT TRACKED ONTO PUBLIC RIGHT-OF-WAYS SHALL BE SWEEP AT THE END OF EACH WORKING DAY.

ALL STOCKPILE AREAS SHALL BE LOCATED WITHIN LIMIT OF WORK LINE AND STABILIZED TO PREVENT EROSION.

ALL DEBRIS GENERATED DURING SITE PREPARATION ACTIVITIES SHALL BE LEGALLY DISPOSED OF OFF SITE.

PROVIDE CROBBING AS NECESSARY TO PROTECT EXISTING UTILITY LINES DURING CONSTRUCTION.

SITE ELEMENTS TO REMAIN MUST BE PROTECTED FOR DURATION OF PROJECT.

ADDITIONAL EROSION CONTROL MEASURES SHALL BE IMPLEMENTED AS CONDITIONS WARRANT OR AS DIRECTED BY THE OWNER OR OWNER'S REPRESENTATIVE.

ALL POINTS OF CONSTRUCTION EGRESS OR INGRESS SHALL BE MAINTAINED TO PREVENT TRACKING OR FLOWING OF SEDIMENT ONTO PUBLIC ROADS.

TEMPORARY DIVERSION DITCHES, PERMANENT DITCHES, CHANNELS, EMBANKMENTS AND ANY DENUDED SURFACE WHICH WILL BE EXPOSED FOR A PERIOD OF ONE MONTH OR MORE SHALL BE CONSIDERED CRITICAL VEGETATION AREAS. THESE AREAS SHALL BE MULCHED WITH STRAW. MULCH SHALL BE SPREAD UNIFORMLY IN A CONTINUOUS BLANKET OF SUFFICIENT THICKNESS TO COMPLETELY HIDE THE SOIL FROM VIEW.

SOIL EROSION AND SEDIMENTATION CONTROL MEASURES SHALL BE INSPECTED AND MAINTAINED ON A DAILY BASIS DURING CONSTRUCTION TO INSURE THAT CHANNELS, DITCHES AND PIPES ARE CLEAR OF DEBRIS AND THAT THE EROSION CONTROL BARRIERS ARE INTACT.

CONTRACTOR SHALL PROVIDE DUST CONTROL FOR CONSTRUCTION OPERATIONS AS APPROVED BY OWNER.

DUST SHALL BE CONTROLLED BY SPRINKLING OR OTHER APPROVED METHODS AS NECESSARY, OR AS DIRECTED BY THE OWNER OR HIS REPRESENTATIVE.

FILTER BAGS SHALL BE INSTALLED IN ALL EXISTING DRAIN INLETS PRIOR TO CONSTRUCTION ACTIVITIES AND AROUND ALL PROPOSED DRAIN INLETS PRIOR TO PERMANENT PAVEMENT TO CONTROL SILTATION.

ADDITIONAL STRAW BALES SHALL BE LOCATED AS CONDITIONS WARRANT OR AS DIRECTED BY THE OWNER OR HIS REPRESENTATIVE.

CLEAN AND MAINTAIN EROSION CONTROL BARRIER AS REQUIRED DURING CONSTRUCTION OPERATIONS TO ENSURE ITS CONTINUED FUNCTIONALITY.

LAYOUT AND MATERIALS NOTES

ALL LINES AND DIMENSIONS ARE PARALLEL OR PERPENDICULAR TO THE LINES FROM WHICH THEY ARE MEASURED UNLESS OTHERWISE INDICATED.

COORDINATE THE LOCATION OF ALL SITE LIGHT STANDARDS WITH IMPROVEMENTS SHOWN ON THESE DRAWINGS.

CONTRACTOR SHALL REPORT SIGNIFICANT CONFLICTS TO THE OWNER, ARCHITECT AND THE ENGINEER FOR RESOLUTION.

DIMENSIONS OF DRIVEWAYS ARE FROM FACE OF CURB TO FACE OF CURB.

DIMENSIONS FROM BUILDING ARE FROM FACE OF BUILDING TO FACE OF CURB. CONTRACTOR SHALL VERIFY ALL CONDITIONS IN THE FIELD AND REPORT ANY DISCREPANCIES TO THE ENGINEER. THE CONTRACTOR SHALL STAKE OUT BUILDING FROM THE LATEST ARCHITECTURAL DRAWINGS.

THE CONTRACTOR SHALL NOTIFY THE ARCHITECT AND ENGINEER OF ANY DISCREPANCIES BETWEEN SITE PLAN DIMENSIONS AND BUILDING PLANS BEFORE PROCEEDING WITH ANY PORTION OF SITE WORK WHICH MAY BE AFFECTED SO THAT PROPER ADJUSTMENTS TO THE SITE LAYOUT CAN BE MADE IF NECESSARY.

SEE ARCHITECTURAL DRAWINGS FOR EXACT BUILDING DIMENSIONS AND ALL DETAIL CONTIGUOUS TO THE BUILDING, LIGHTING, ENTRANCE PATIO, DOORWAY PADS, LOADING DOCK DETAILS, ETC.

ACCESSIBLE RAMPS SHALL BE PER MASSACHUSETTS STATE CODE AND THE AMERICANS WITH DISABILITIES ACT (ADA) ACCESSIBILITY GUIDELINES (WHICHEVER IS MORE STRINGENT).

EACH HANDICAP PARKING SPACE SHALL BE IDENTIFIED BY A SIGN SIX (6) FEET IN HEIGHT LOCATED AT THE FACE OF THE CURBLINE. THE SIGN SHALL CONTAIN THE INTERNATIONAL SYMBOL OF ACCESSIBILITY AS DESCRIBED IN THE AMERICANS WITH DISABILITIES ACT, PUBLIC LAW 101-336, (SEE DETAIL).

PROTECT EXISTING PROPERTY MONUMENTS AND ABUTTING PROPERTIES DURING CONSTRUCTION ACTIVITIES.

GRADING, DRAINAGE AND UTILITY NOTES

UNDERGROUND UTILITIES WERE COMPILED FROM AVAILABLE RECORD PLANS OF UTILITY COMPANIES AND PUBLIC AGENCIES, ARE APPROXIMATE AND ASSUMED. BEFORE COMMENCING SITE WORK IN ANY AREA, CONTACT "DIG SAFE" AT 1-888-344-7233 TO ACCURATELY LOCATE UNDERGROUND UTILITIES. ANY DAMAGE TO EXISTING UTILITIES OR STRUCTURES SHALL BE THE CONTRACTOR'S RESPONSIBILITY. NO EXCAVATION SHALL BE DONE UNTIL UTILITY COMPANIES ARE PROPERLY NOTIFIED IN ADVANCE.

ALL SITE WORK SHALL MEET OR EXCEED THE SITE WORK SPECIFICATIONS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING THAT THE PROPOSED IMPROVEMENTS SHOWN ON THE PLANS DO NOT CONFLICT WITH ANY KNOWN EXISTING OR OTHER PROPOSED IMPROVEMENTS. IF ANY CONFLICTS ARE DISCOVERED, THE CONTRACTOR SHALL NOTIFY THE OWNER, ARCHITECT AND THE ENGINEER PRIOR TO INSTALLATION OF ANY PORTION OF THE SITE WORK WHICH WOULD BE AFFECTED.

ALL WORK PERFORMED AND ALL MATERIALS FURNISHED SHALL CONFORM WITH THE LINES, GRADES AND OTHER SPECIFIC REQUIREMENTS OR SPECIFICATIONS OF THE CITY OF CAMBRIDGE AND CITY OF SOMERVILLE DPW.

AT ALL LOCATIONS WHERE EXISTING CURBING OR PAVEMENT ABUTS NEW CONSTRUCTION, THE EDGE OF THE EXISTING CURB OR PAVEMENT SHALL BE SAW CUT TO A CLEAN, SMOOTH EDGE. BLEND NEW PAVEMENT, CURBS AND EARTHWORK SMOOTHLY INTO EXISTING BY MATCHING LINES, GRADES AND JOINTS. PITCH EVENLY BETWEEN SPOT GRADES. GRADE ALL AREAS TO DRAIN.

THE CONTRACTOR SHALL VERIFY EXISTING GRADES IN THE FIELD AND REPORT ANY DISCREPANCIES IMMEDIATELY TO THE ENGINEER AND ARCHITECT. THE CONTRACTOR SHALL MAKE ALL ARRANGEMENTS FOR THE ALTERATION AND ADJUSTMENT OF GAS, ELECTRIC, TELEPHONE AND ANY OTHER PRIVATE UTILITIES BY THE UTILITY COMPANIES, AS REQUIRED. WHERE AN EXISTING UTILITY IS FOUND TO CONFLICT WITH THE PROPOSED WORK, THE LOCATION, ELEVATION AND SIZE OF THE UTILITY SHALL BE ACCURATELY DETERMINED WITHOUT DELAY BY THE CONTRACTOR, AND THE INFORMATION FURNISHED TO THE OWNER, ARCHITECT AND ENGINEER FOR RESOLUTION.

ALL UTILITY COVERS, GRATES, ETC. SHALL BE ADJUSTED TO BE FLUSH WITH THE PAVEMENT FINISH GRADE UNLESS OTHERWISE NOTED. RIM ELEVATIONS OF DRAINAGE STRUCTURES AND SANITARY SEWER MANHOLES ARE APPROXIMATE.

CONTRACTOR SHALL PROTECT ALL UNDERGROUND DRAINAGE, SEWER AND UTILITY FACILITIES FROM EXCESSIVE VEHICULAR LOADS DURING CONSTRUCTION. ANY DAMAGE TO THESE FACILITIES RESULTING FROM CONSTRUCTION LOADS WILL BE RESTORED TO ORIGINAL CONDITION.

EXCAVATION REQUIRED WITHIN THE PROXIMITY OF EXISTING UTILITY LINES SHALL BE DONE BY HAND. CONTRACTOR SHALL REPAIR ANY DAMAGE TO EXISTING UTILITY LINES OR STRUCTURES INCURRED DURING CONSTRUCTION OPERATIONS AT NO COST TO THE OWNER.

PITCH EVENLY BETWEEN SPOT GRADES. ALL PAVED AREAS MUST PITCH TO DRAIN AT A MINIMUM OF 1/8" PER FOOT UNLESS SPECIFIED. ANY DISCREPANCIES NOT ALLOWING THIS MINIMUM PITCH SHALL BE REPORTED TO THE ENGINEER PRIOR TO CONTINUING WORK.

THE CONTRACTOR SHALL SCHEDULE HIS WORK TO ALLOW THE FINISHED SUBGRADE ELEVATIONS TO DRAIN PROPERLY WITHOUT PULDING. SPECIFICALLY, ALLOW WATER TO ESCAPE WHERE PROPOSED CURB MAY RETAIN RUNOFF PRIOR TO APPLICATION OF THE FINISH SUBGRADE AND/OR SURFACE PAVING. PROVIDE TEMPORARY POSITIVE DRAINAGE AS REQUIRED.

UNLESS OTHERWISE INDICATED, ABANDONED EXISTING UTILITY LINES SHALL BE CAPPED AND ABANDONED IN PLACE UNLESS THEY CONFLICT WITH PROPOSED IMPROVEMENTS. CAP REMAINING PORTIONS WHERE PARTIALLY REMOVED.

LEGEND AND ABBREVIATIONS

EXISTING	PROPOSED		EXISTING	PROPOSED	
		SEWER LINE/MANHOLE			MINOR CONTOUR
		DRAIN LINE			MAJOR CONTOUR
		ROOF DRAIN LINE			SPOT ELEVATION
		CATCH BASIN			AREA DRAIN
		FLARED END/INVERT			BITUMINOUS CONCRETE
		GAS LINE/GATE			BENCHMARK
		WATER LINE/GATE			CABLE TELEVISION
		HYDRANT			CEMENT LINED DUCTILE IRON
		TELEPHONE LINE/MANHOLE			CONCRETE
		ELECTRIC LINE/MANHOLE			EDGE OF PAVEMENT
		OVERHEAD WIRE			GAS METER
		LIGHT POLE			HEAD WALL
		UTILITY POLE			LOADING DOCK
		GUY WIRE			POLYVINYL CHLORIDE
		SIGN			REINFORCED CONCRETE PIPE
		BOLLARD POST			ROOF DRAIN
		HAND HOLE			RIGHT-OF-WAY
		CHAIN LINK FENCE			SLOPED GRANITE CURB
		GRANITE CURB			ELECTRIC TRANSFORMER
		BITUMINOUS CONCRETE BERM			WATER QUALITY INLET
		TREE			STONE BOUND
		BUILDING			CONCRETE BOUND
					DRILL HOLE
					IRON PIN/IRON PIPE
					IRON ROD
					FOUND

DRAWING LIST

C1.0	NOTES, REFERENCES AND LEGEND
TP-1	TOPOGRAPHIC PLAN
C2.0	LAYOUT AND MATERIALS PLAN
C3.0	GRADING, DRAINAGE AND UTILITY PLAN
C4.0	SITE DETAILS
C4.1	SITE DETAILS

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Somerville, MA, 02141

Design & Site Plan Submission

BEALS + THOMAS
Civil and Survey
Reservoir Corporate Center
144 Turnpike Road
Southborough, MA 01772
Phone: 508.366.0560

Stamp

NOT FOR CONSTRUCTION

Revisions		
No.	Description	Date

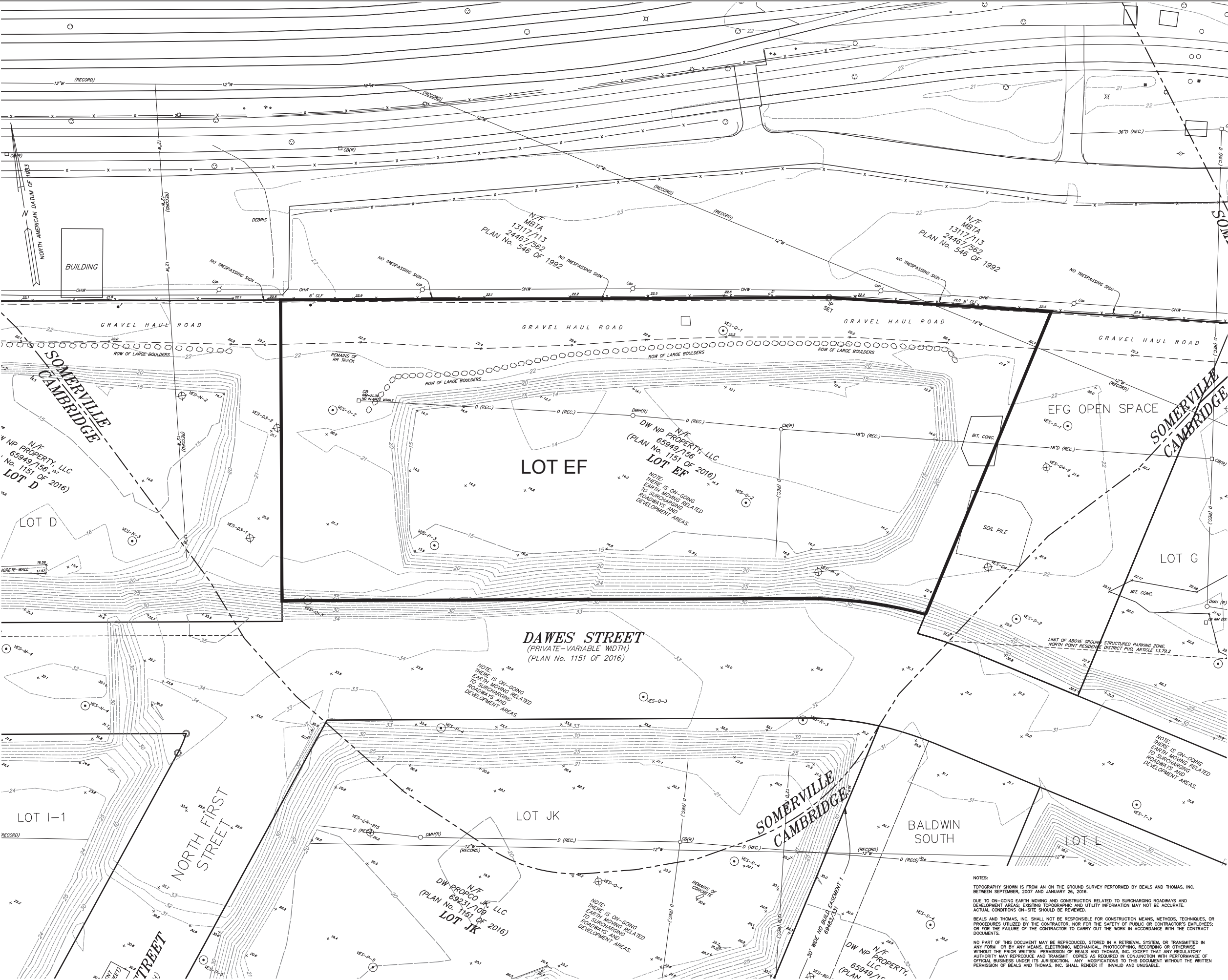
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Notes, References and
Legend

B&T DWG. NO. 208429P2908-001

Job Number	15039-05	Drawing No.
Drawn by	B&T	
Checked	B&T	
Date	30 November 2017	
Scale	N/A	

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Somerville, MA, 02141

Design & Site Plan Submission

BEALS + THOMAS

Civil and Survey
Reservoir Corporate Center
144 Turnpike Road
Southborough, MA 01772
Phone: 508.366.0560

Stamp

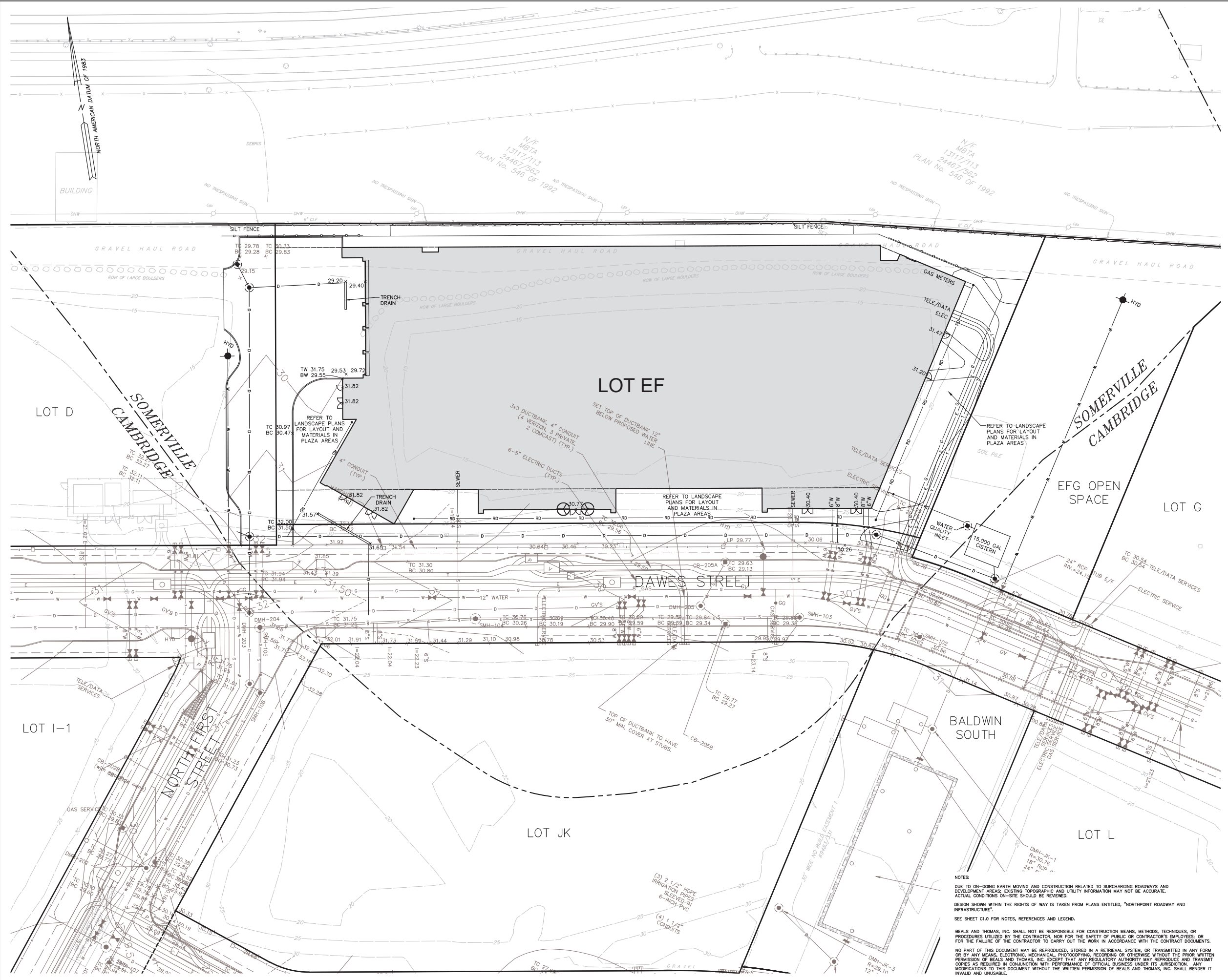
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Revisions		
No.	Description	Date

Drawing Title
Topographic Plan

B&T DWG. NO. 208422P22918-001	Drawing No.
Job Number 15039-05	Drawn by B&T
Checked B&T	
Date 30 November 2017	TP-1
Scale 1" = 20'	

NOTES:
TOPOGRAPHY SHOWN IS FROM AN ON THE GROUND SURVEY PERFORMED BY BEALS AND THOMAS, INC. BETWEEN SEPTEMBER, 2007 AND JANUARY 26, 2016.
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Revisions		
No.	Description	Date

Drawing Title
Grading, Drainage and Utilities Plan

B&T DWG. NO. 208429P292B-002

Job Number	15039-05	Drawing No.
Drawn by	B&T	
Checked	B&T	
Date	30 November 2017	
Scale	1" = 20'	

C3.0

NOTES:

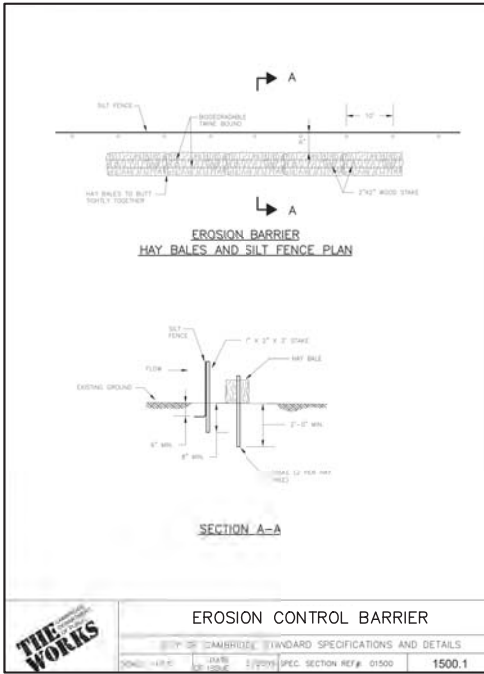
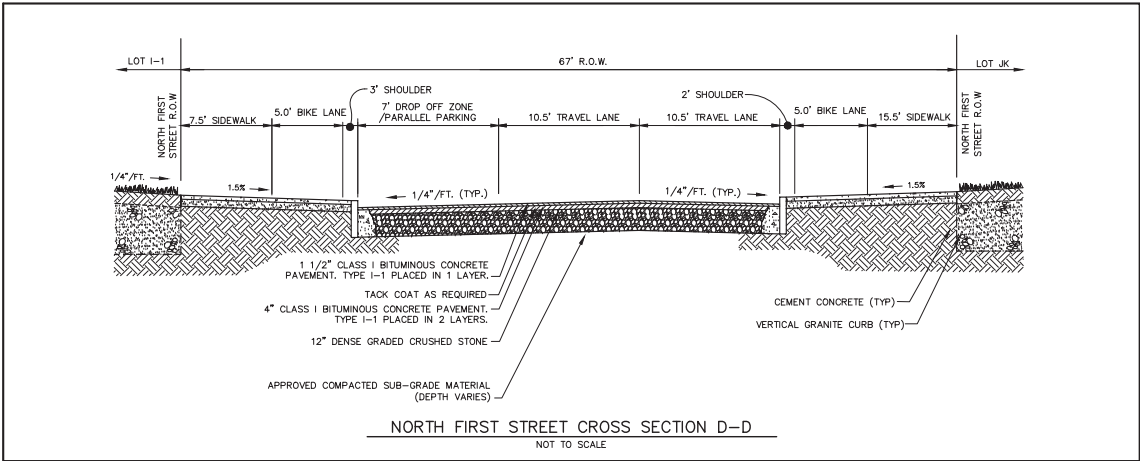
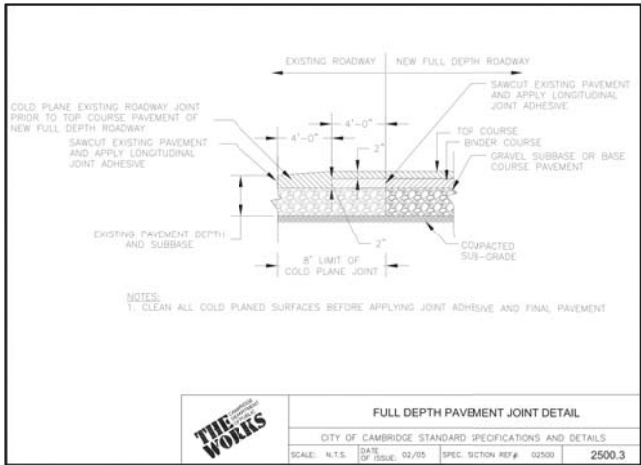
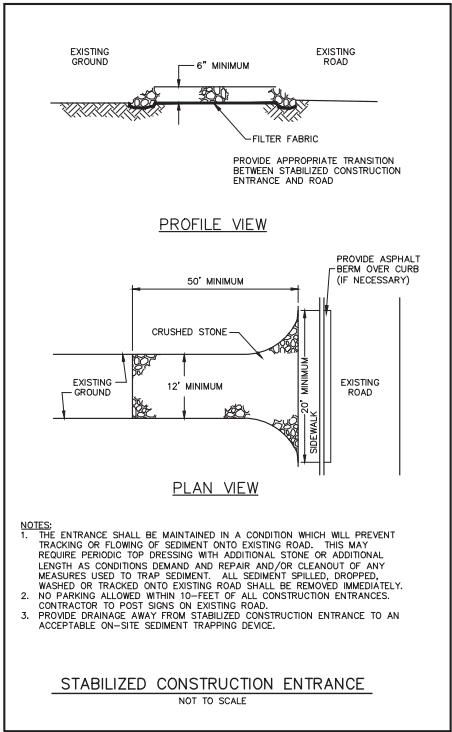
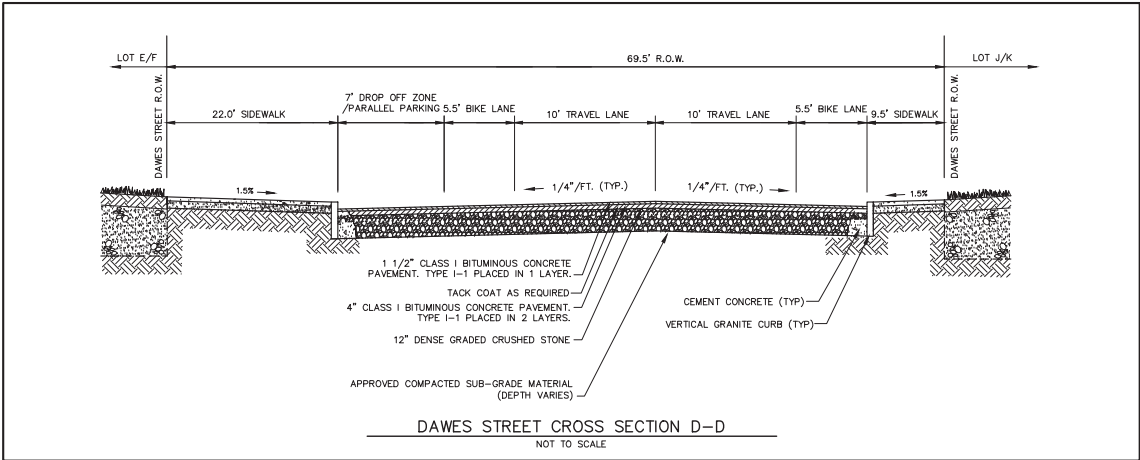
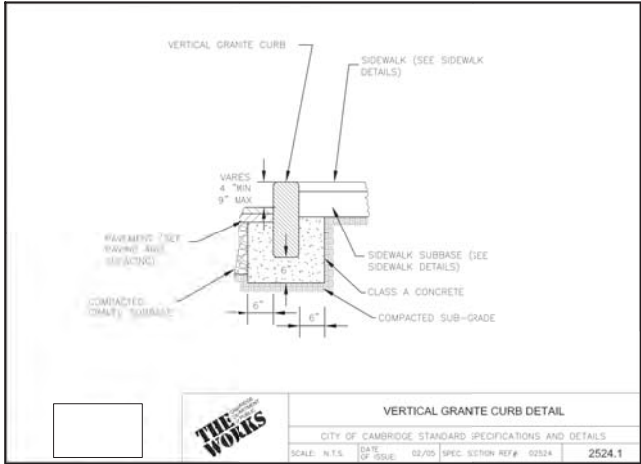
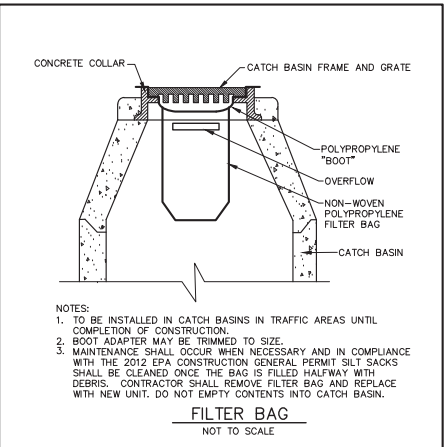
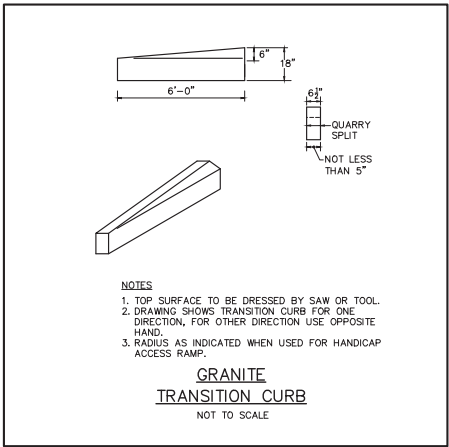
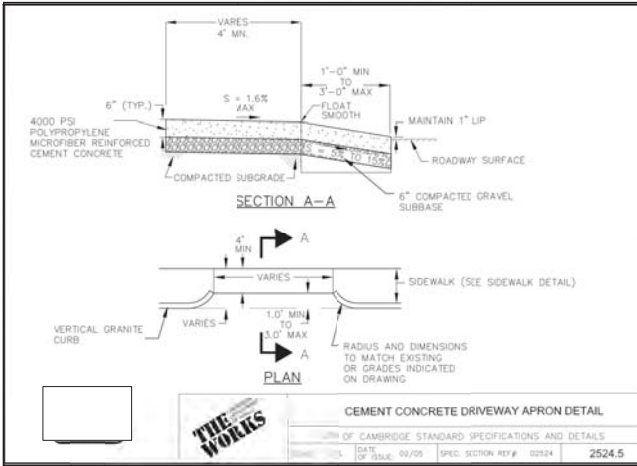
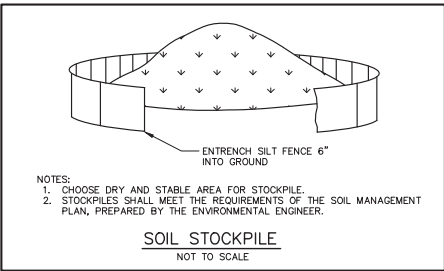
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DESIGN SHOWN WITHIN THE RIGHTS OF WAY IS TAKEN FROM PLANS ENTITLED, "NORTHPOINT ROADWAY AND INFRASTRUCTURE".

SEE SHEET C1.0 FOR NOTES, REFERENCES AND LEGEND.

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Revisions	Description	Date
No.		

Drawing Title
Site Details

B&T DWG. NO. 208429P2908-002

Job Number 15039-05 Drawing No.

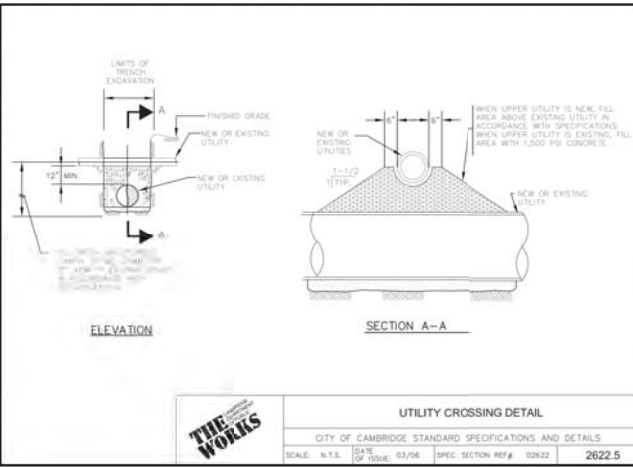
Drawn by B&T

Checked B&T

Date 30 November 2017

Scale Not To Scale

C4.0



GENERAL NOTES

THE CONTRACTOR IS RESPONSIBLE FOR THE PROTECTION OF THE EXISTING FEATURES AND STRUCTURES WITHIN AND ADJACENT TO THE WORK. IN THE CASE OF DAMAGE, THE REPAIRS OR REPLACEMENT SHALL BE PERFORMED AT THE CONTRACTOR'S EXPENSE TO THE SATISFACTION OF THE OWNER.

THE CONTRACTOR SHALL FURNISH ALL PROTECTIVE AND/OR WARNING DEVICES AS REQUIRED TO CONTROL AND PROTECT THE PUBLIC IN ALL WORK AREAS.

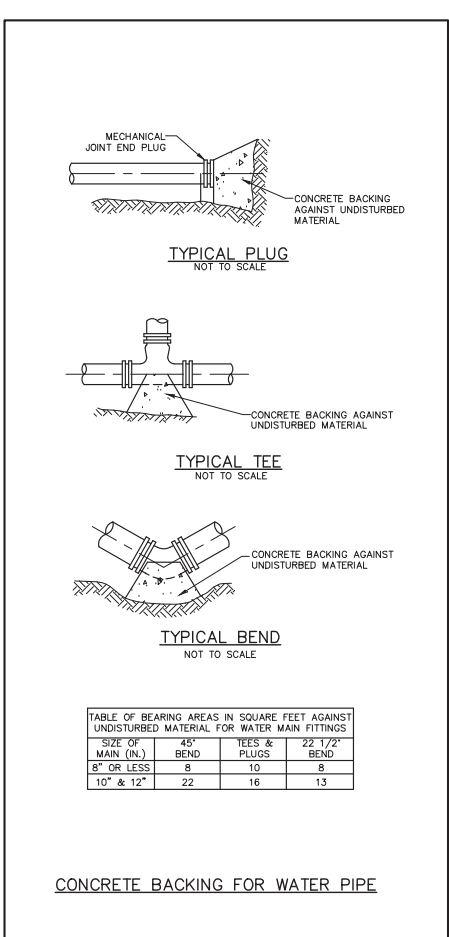
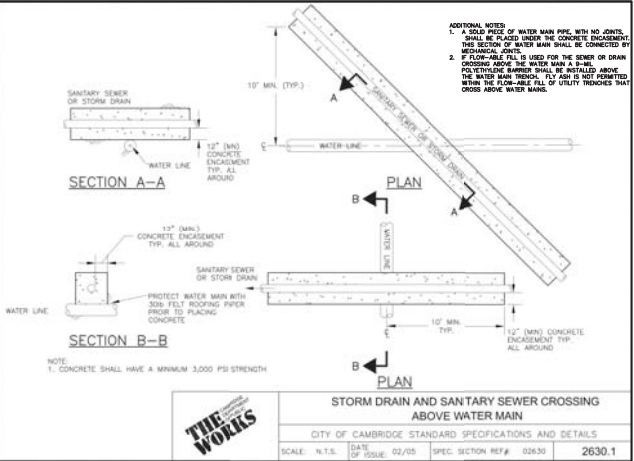
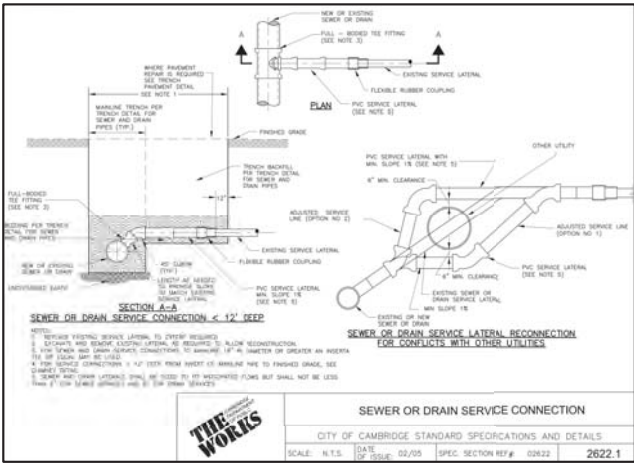
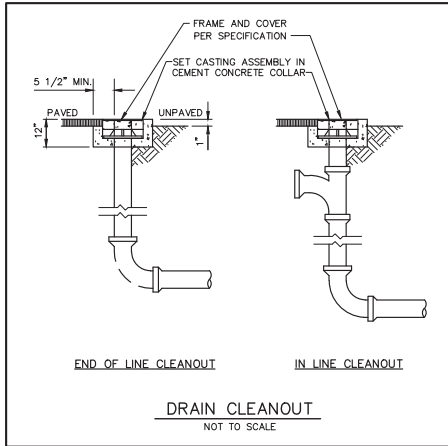
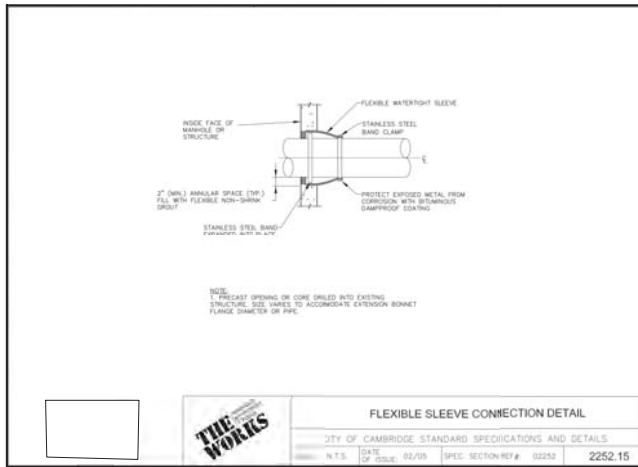
PIPE BEDDING: SCREENED GRAVEL AND/OR CRUSHED STONE FREE FROM CLAY, LOAM, ORGANIC MATTER AND MEETING ASTM C33 STONE SIZE NO. 67.

PARTICLE SIZE	% PASSING
1 INCH SCREEN	100
3/4 INCH SCREEN	90-100
3/8 INCH SCREEN	20-50
#4 SIEVE	0-10
#6 SIEVE	0-5

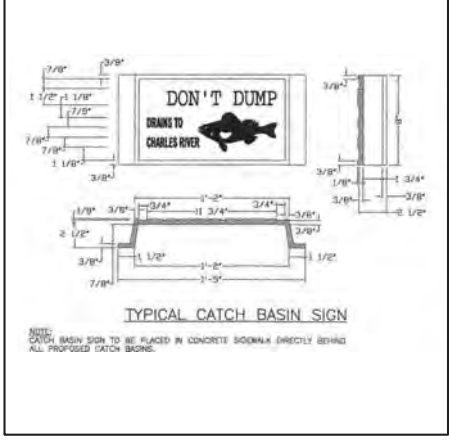
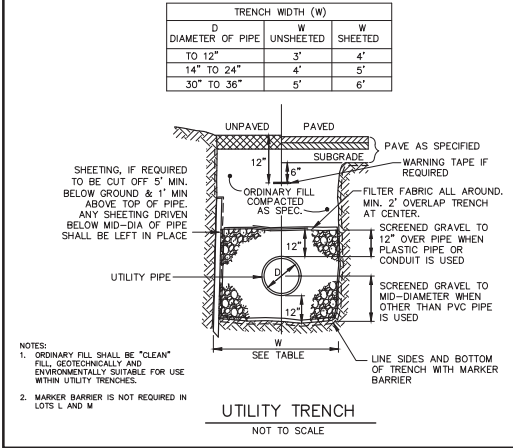
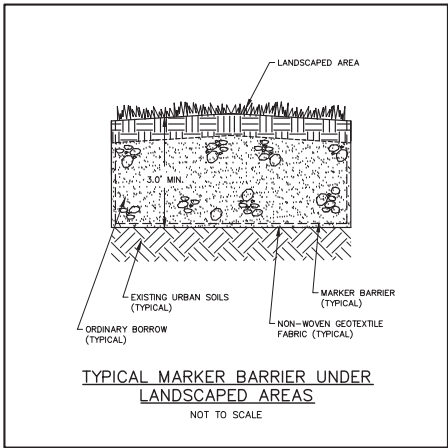
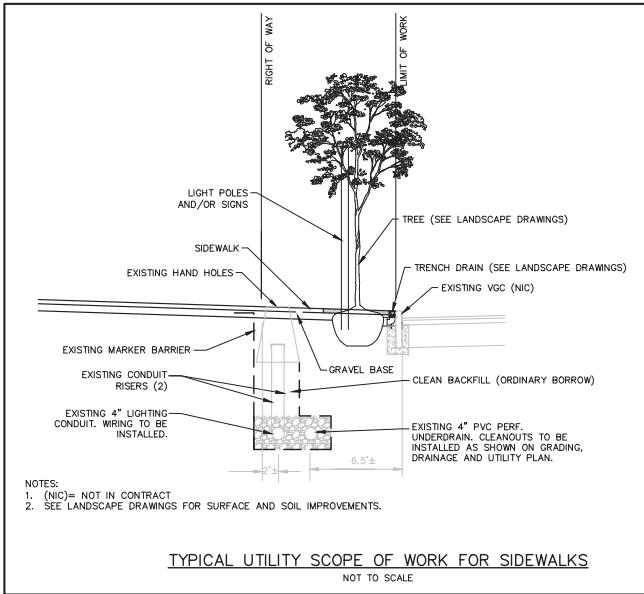
WHERE DIRECTED BY THE ENGINEER TO STABILIZE THE TRENCH BASE, GRADED SCREENED GRAVEL OR CRUSHED STONE 1/2 INCH TO 1-1/2 INCH SHALL BE USED.

SAND BLANKET: CLEAN SAND FREE FROM ORGANIC MATTER, SO GRADED THAT 90-100% PASSES 1/2 INCH SIEVE AND NOTE MORE THAN 15% PASSES #200 SIEVE.

CONCRETE ENCASMENT, IF REQUIRED, SHALL CONFORM TO THE REQUIREMENTS FOR 3,000 PSI CONCRETE.



SIZE OF MAIN (IN)	45° BEND	TEES & PLUGS	22 1/2° BEND
8" OR LESS	8	10	8
10" & 12"	22	16	13



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B&T DWG. NO. 208429P2908-003

Job Number 15039-05 Drawing No.

Drawn by B&T

Checked B&T

Date 30 November 2017

Scale Not To Scale

C4.1

PEDESTRIAN WIND CRITERIA

The BRA has adopted two standards for assessing the relative wind comfort of pedestrians. First, the BRA wind design guidance criterion states that an effective gust velocity (hourly mean wind speed +1.5 times the root-mean-square wind speed) of 31 mph should not be exceeded more than one percent of the time. The second set of criteria used by the BRA to determine the acceptability of specific locations is based on the work of Melbourne¹. This set of criteria is used to determine the relative level of pedestrian wind comfort for activities such as sitting, standing, or walking. The criteria are expressed in terms of benchmarks for the 1-hour mean wind speed exceeded 1% of the time (i.e., the 99-percentile mean wind speed). They are as follows:

BRA Mean Wind Criteria*

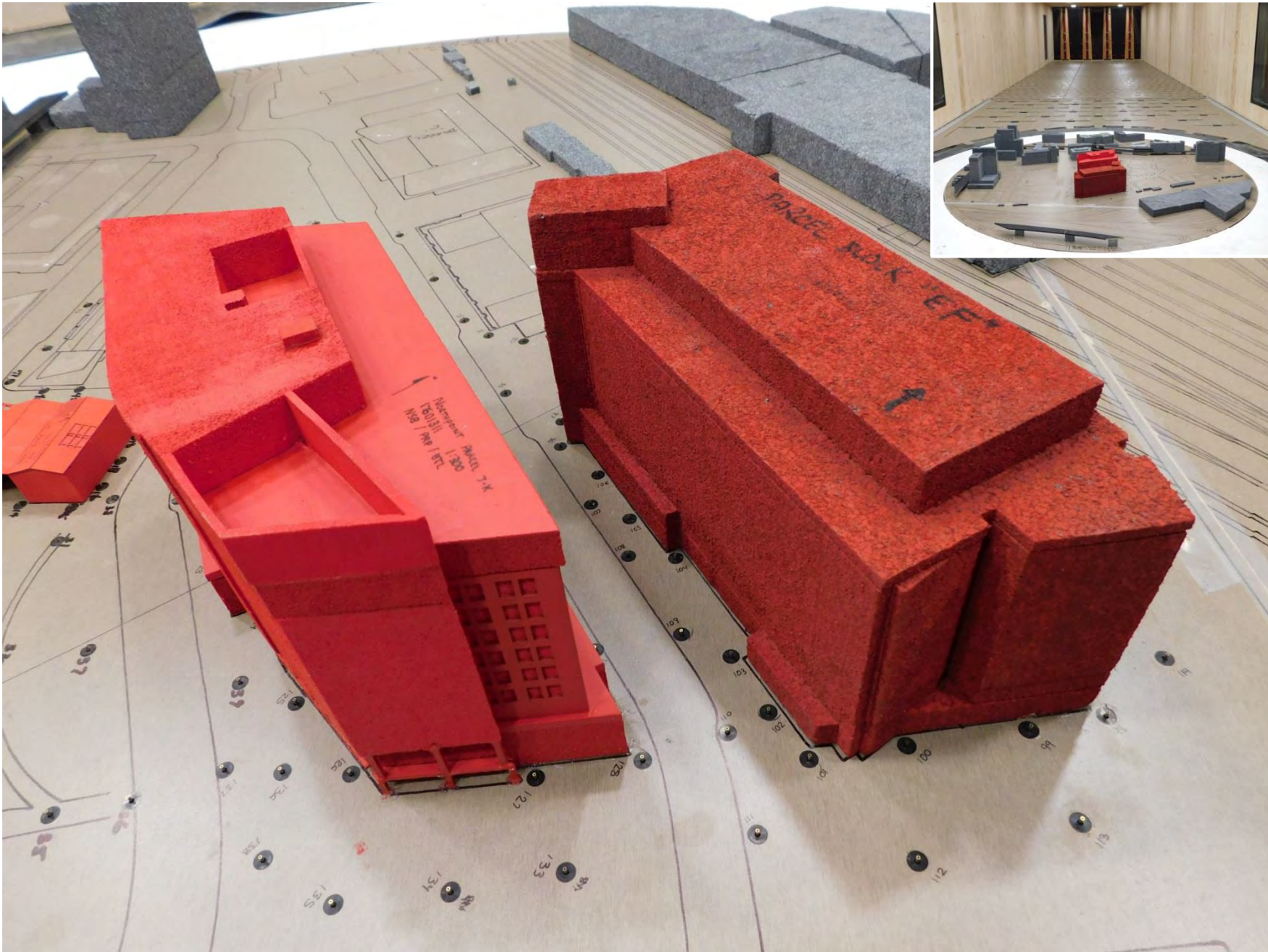
Dangerous	> 27 mph
Uncomfortable for Walking	> 19 and ≤ 27 mph
Comfortable for Walking	> 15 and ≤ 19 mph
Comfortable for Standing	> 12 and ≤ 15 mph
Comfortable for Sitting	< 12 mph

* Applicable to the hourly mean wind speed exceeded one percent of the time.

The wind climate found in a typical downtown location in Boston is generally comfortable for the pedestrian use of sidewalks and thoroughfares and meets the BRA effective gust velocity criterion of 31 mph. However, without any mitigation measures, this wind climate is likely to be frequently uncomfortable for more passive activities such as sitting.

¹ Melbourne, W.H., 1978, "Criteria for Environmental Wind Conditions", Journal of Industrial Aerodynamics, 3 (1978) 241 - 249.

FIGURES



Wind Tunnel Study Model

Build – Parcels E+F

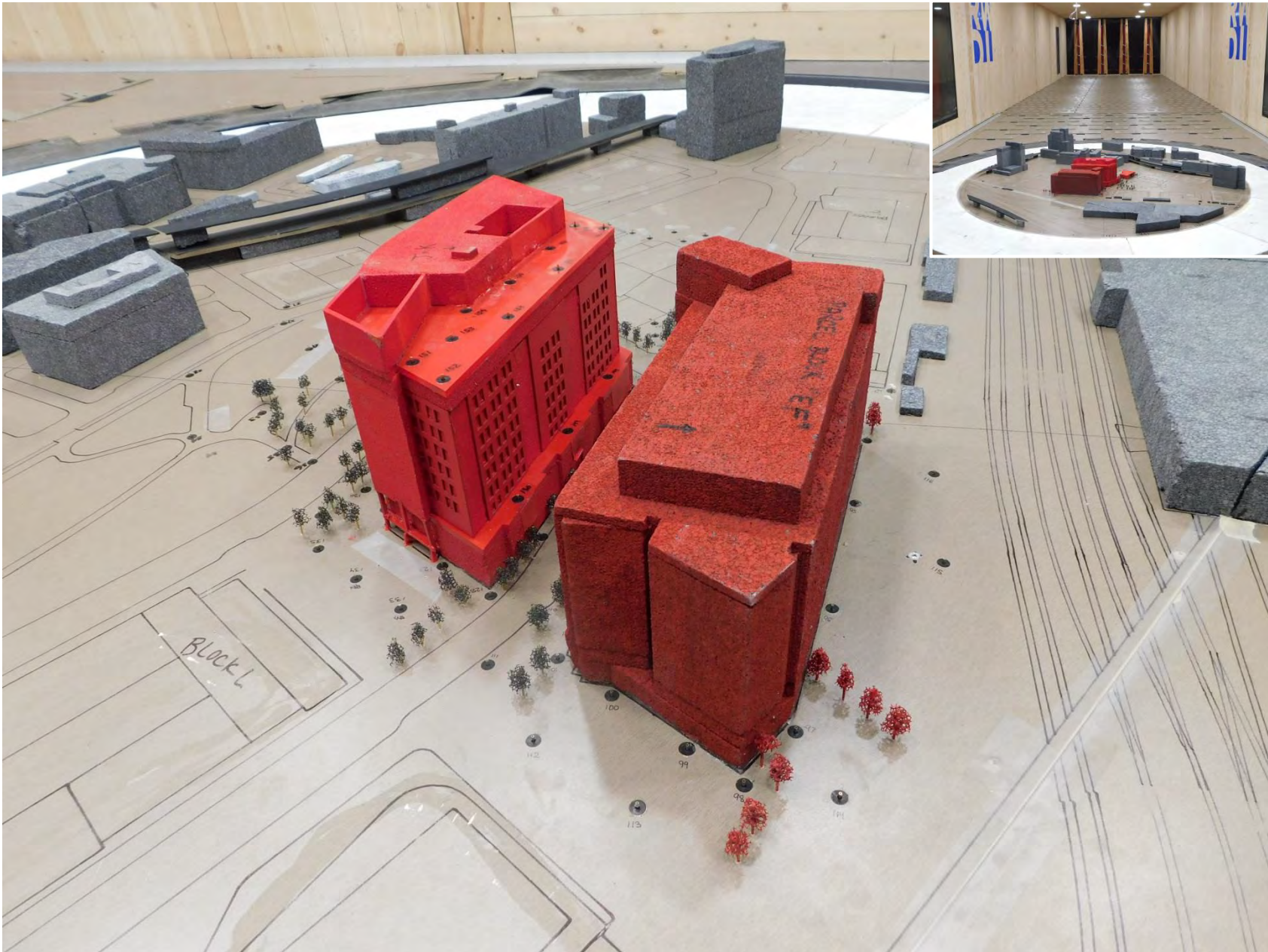
Northpoint – Parcels W, I, E+F and J+K – Cambridge, MA

Figure No. 1a

Project #1700383

Date: October 5, 2016





Wind Tunnel Study Model
 Build – Parcels E+F with Mitigation

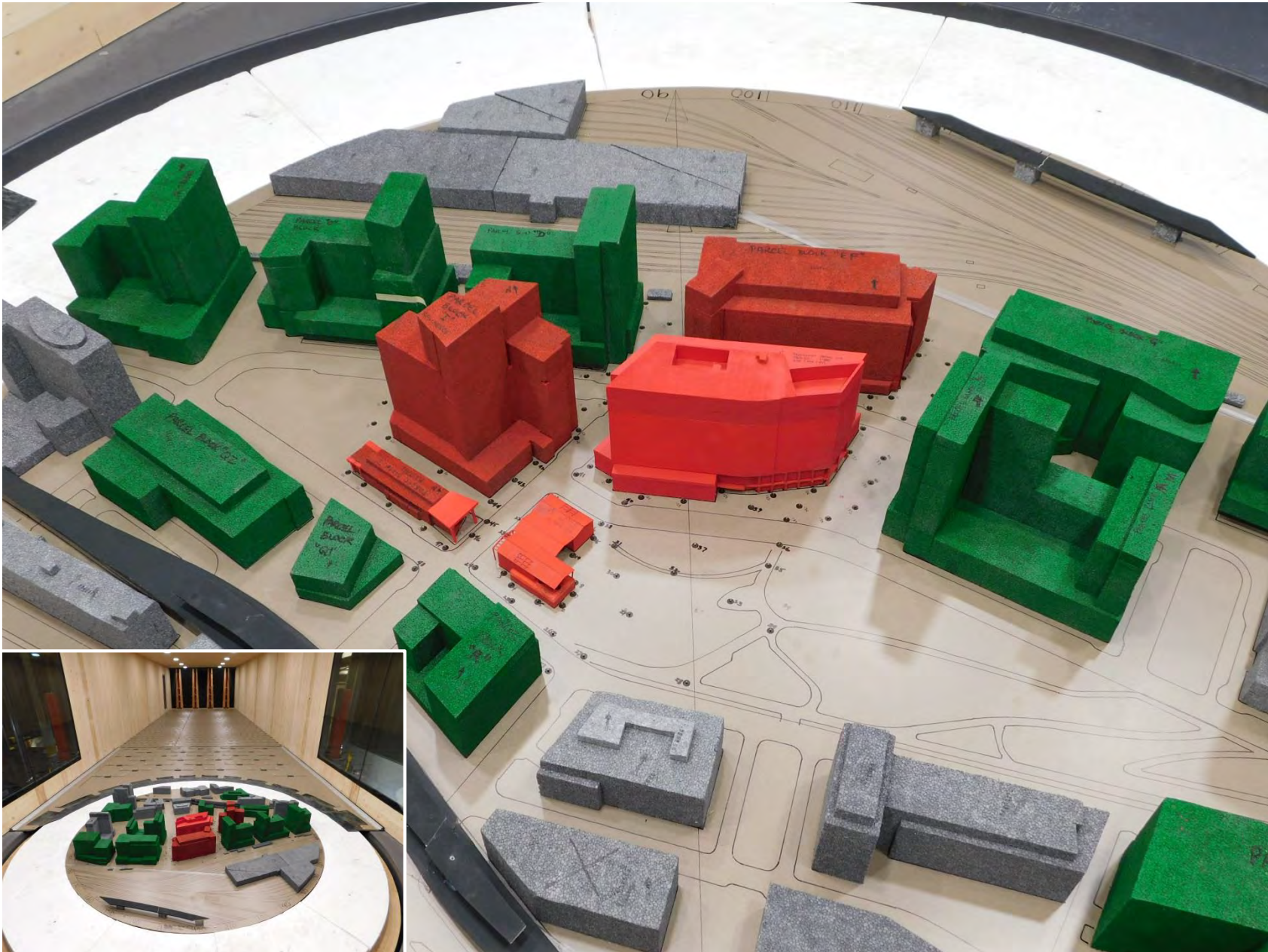
Northpoint – Parcels W, I, E+F and J+K – Cambridge, MA

Figure No. 1b

Date: October 5, 2016



Project #1700383



Wind Tunnel Study Model Full Build

Northpoint – Parcels W, I, E+F, and J+K – Cambridge, MA

Figure No. 1c

Project #1700383

Date: October 5, 2016





Wind Tunnel Study Model

Full Build with Mitigation

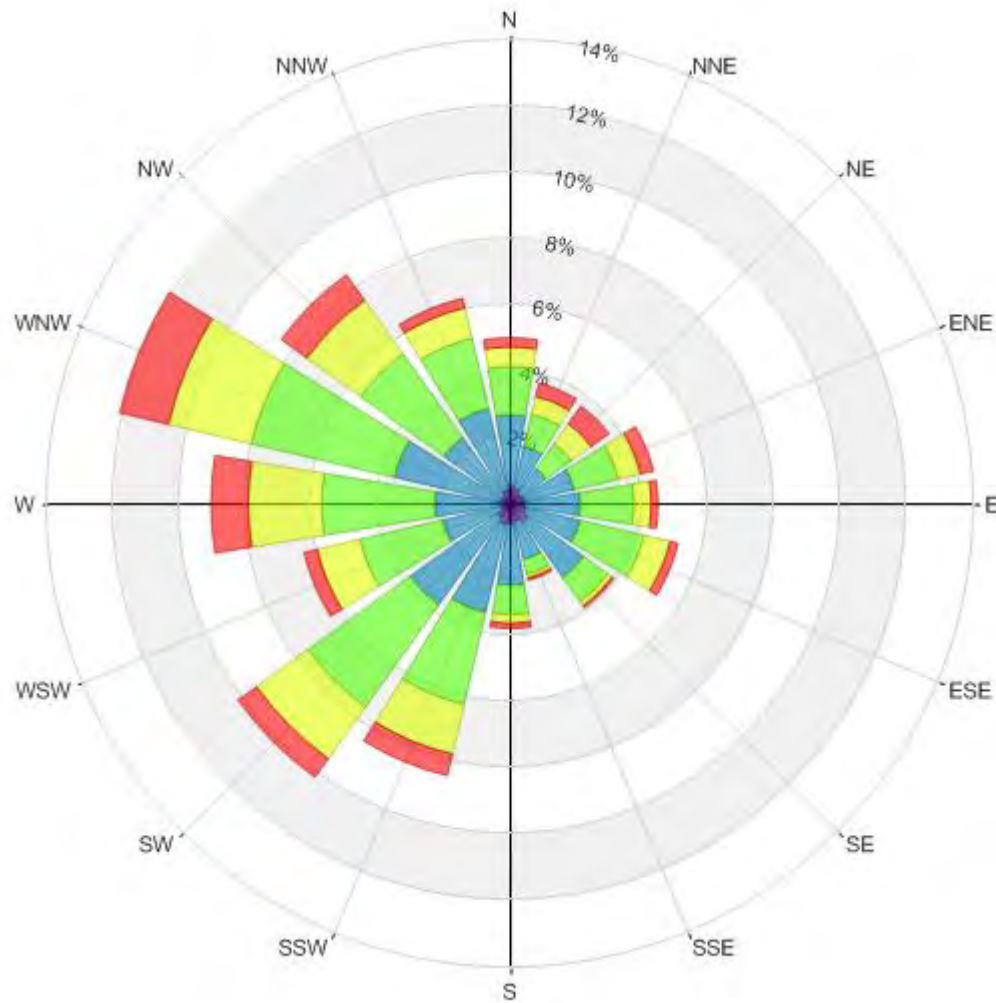
Northpoint – Parcels W, I, E+F, and J+K – Cambridge, MA

Figure No. 1d

Project #1700383

Date: October 5, 2016





Annual Winds

**Wind Speed
(mph)**



Probability (%)

Calm	2.5
1-5	7.4
6-10	32.1
11-15	33.0
16-20	16.9
>20	8.1

**Directional Distribution (%) of Winds (Blowing From)
Boston Logan International Airport (1991 - 2015)**

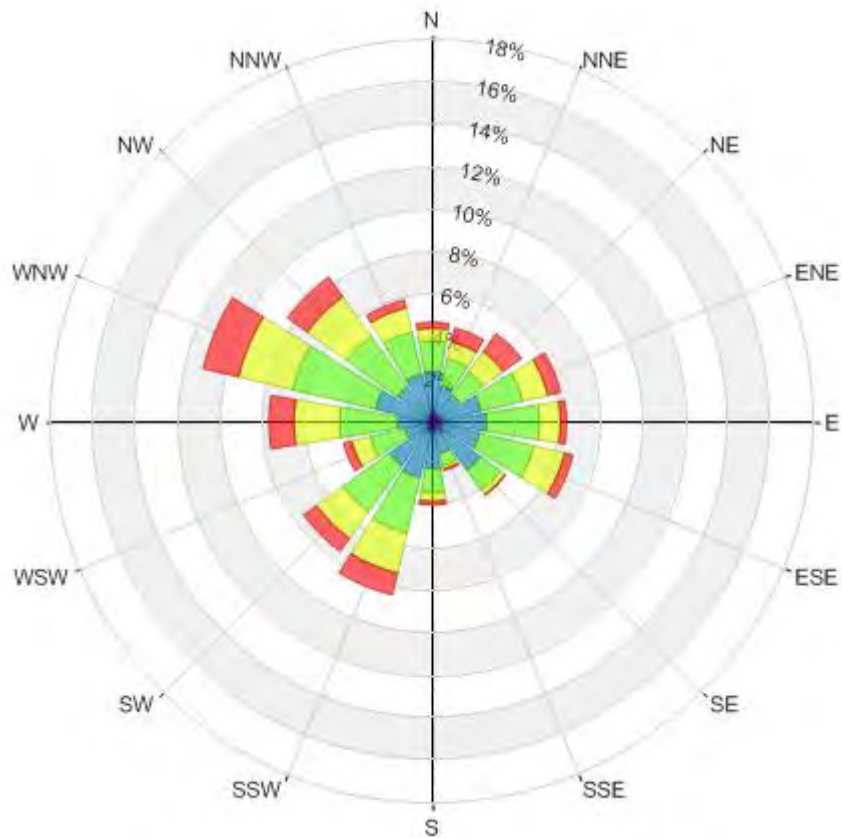
Northpoint - Parcels W, I, E+F and J+K – Cambridge, MA

Figure No. 2

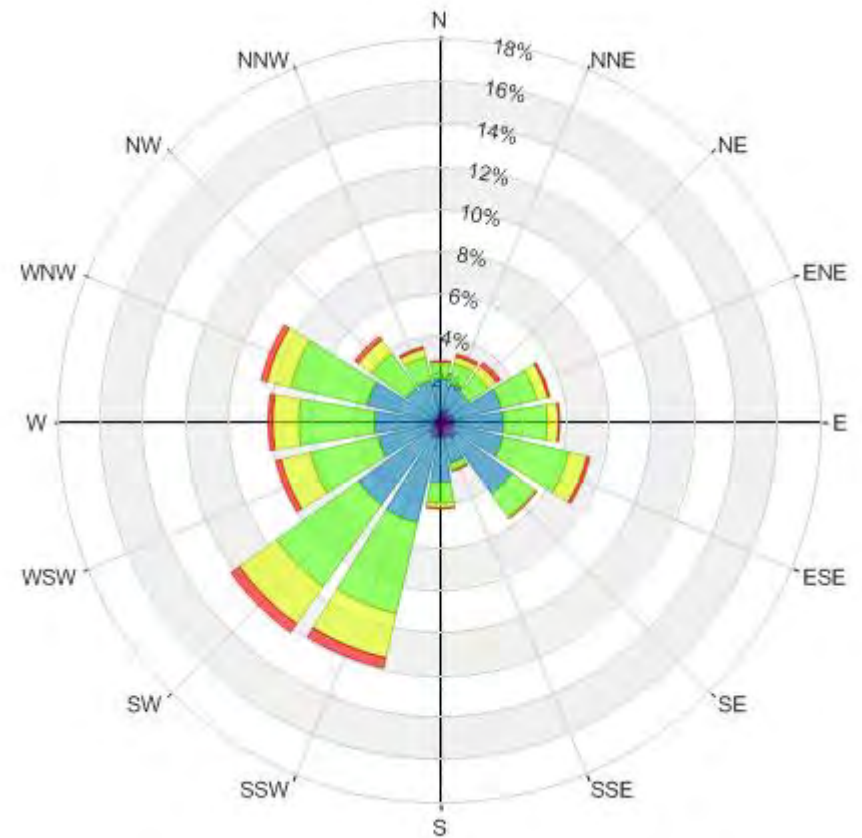
Project #1700383

Date: September 26, 2016





Spring
(March - May)



Summer
(June - August)

	Wind Speed (mph)	Probability (%)	
		Spring	Summer
	Calm	2.4	2.7
	1-5	6.4	8.9
	6-10	28.5	38.1
	11-15	32.9	35.1
	16-20	19.7	12.6
	>20	10.2	2.7

Directional Distribution (%) of Winds (Blowing From) **Boston Logan International Airport (1991 - 2015)**

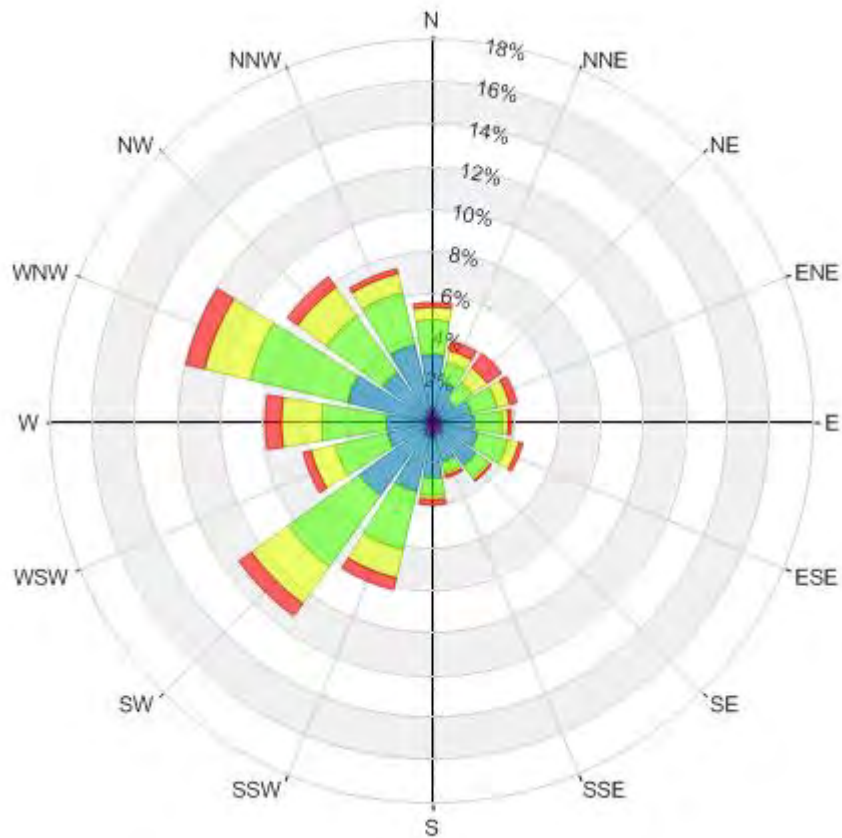
Northpoint - Parcels W, I, E+F and J+K - Cambridge, MA

Project #1700383

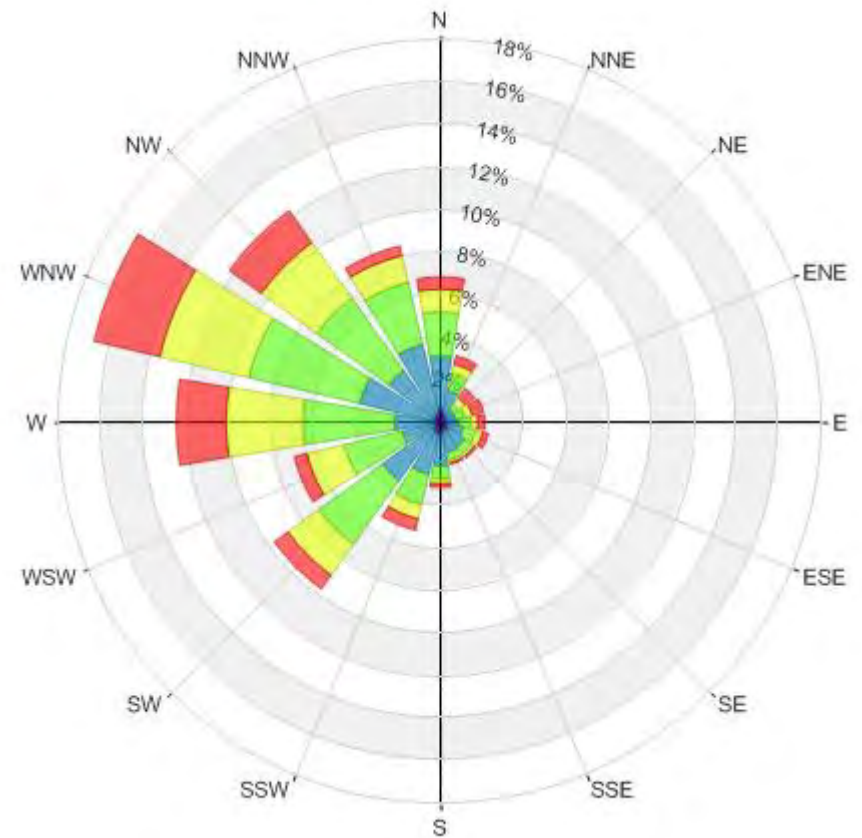
Figure No. 2

Date: Oct. 5, 2016





Fall
(September - November)



Winter
(December - February)

	Wind Speed (mph)	Probability (%)	
		Fall	Winter
	Calm	2.9	2.3
	1-5	8.0	6.2
	6-10	34.3	27.5
	11-15	32.8	31.1
	16-20	15.3	20.1
	>20	6.7	12.8

Directional Distribution (%) of Winds (Blowing From) **Boston Logan International Airport (1991 - 2015)**

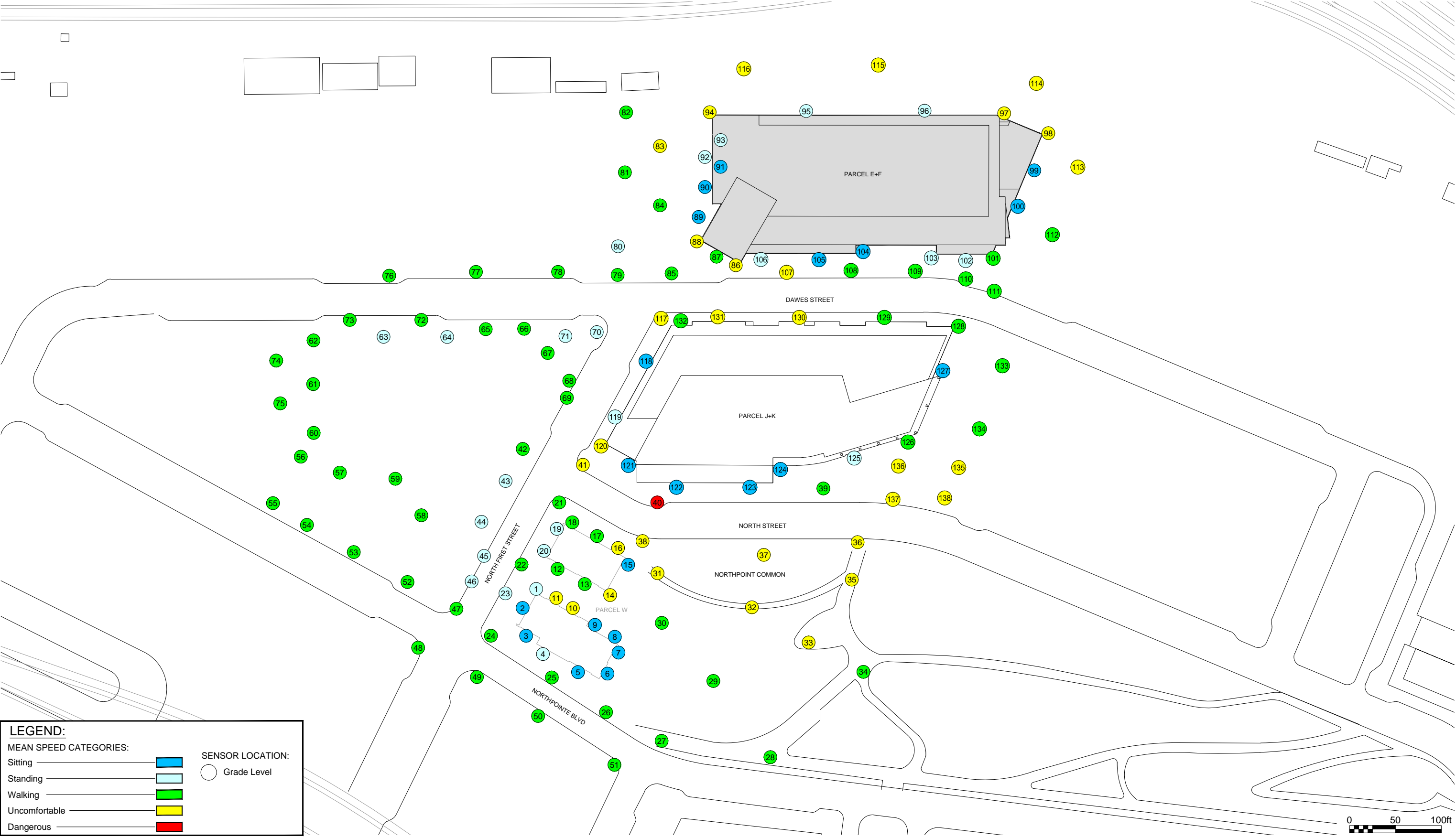
Northpoint Parcels W, I, E+F and J+K – Cambridge, MA

Project #1700383

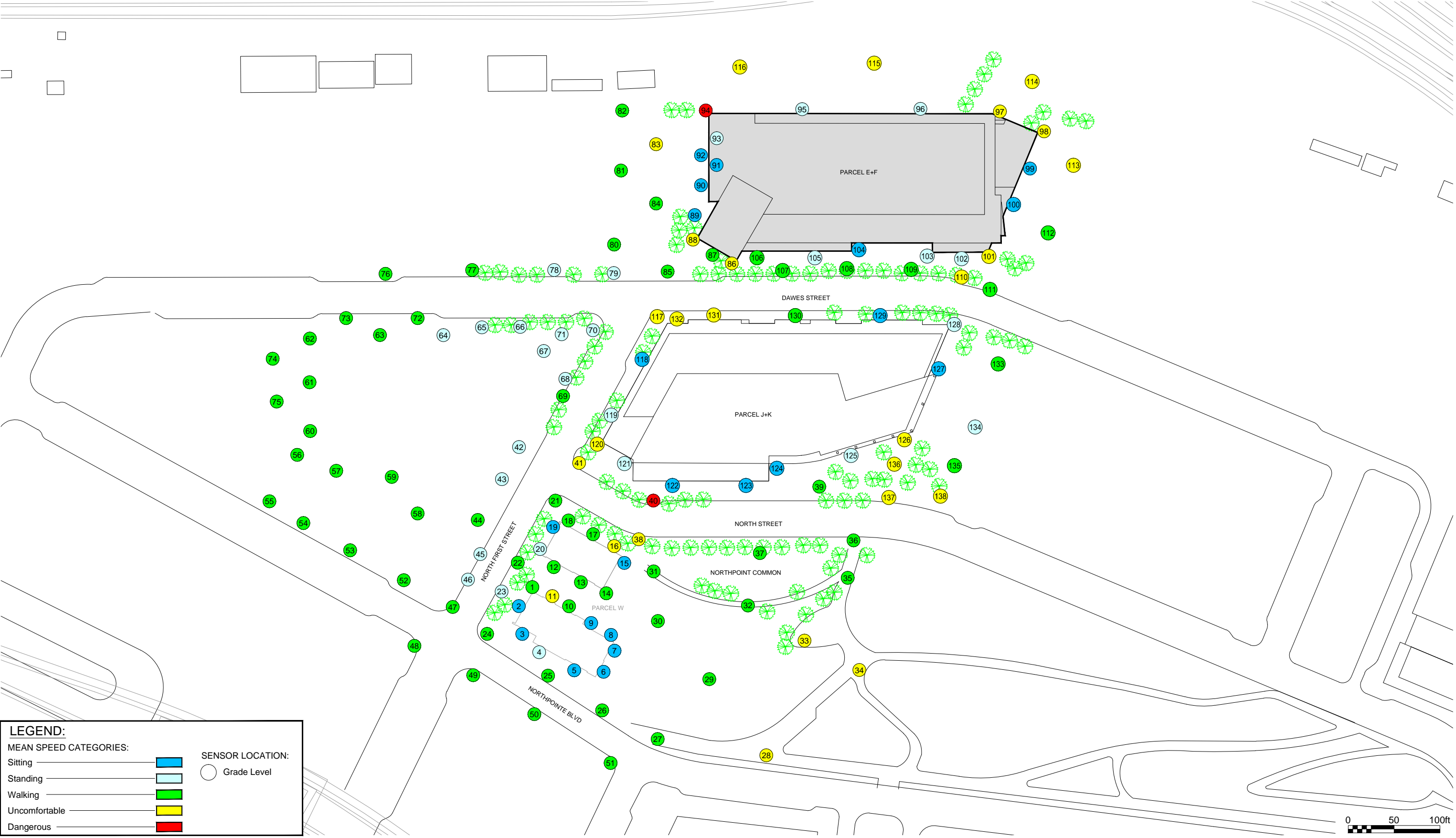
Figure No. 2

Date: Oct. 5, 2016





Pedestrian Wind Conditions - Mean Wind Speed - Parcels E+F
Annual (January to December, 6:00 to 23:00) Comfort



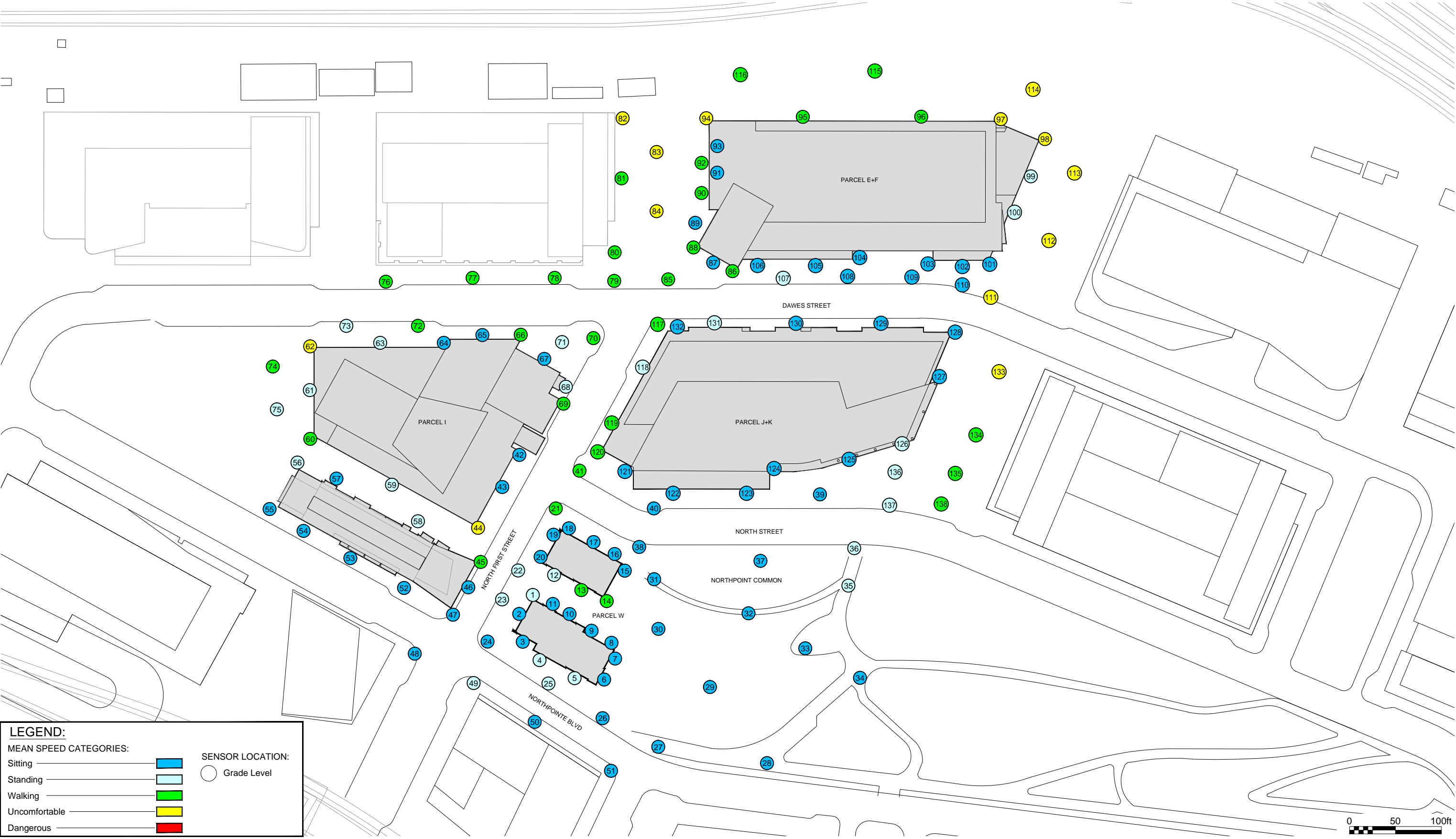
LEGEND:

MEAN SPEED CATEGORIES:

Sitting	Blue
Standing	Light Blue
Walking	Green
Uncomfortable	Yellow
Dangerous	Red

SENSOR LOCATION:

Grade Level	White Circle
-------------	--------------

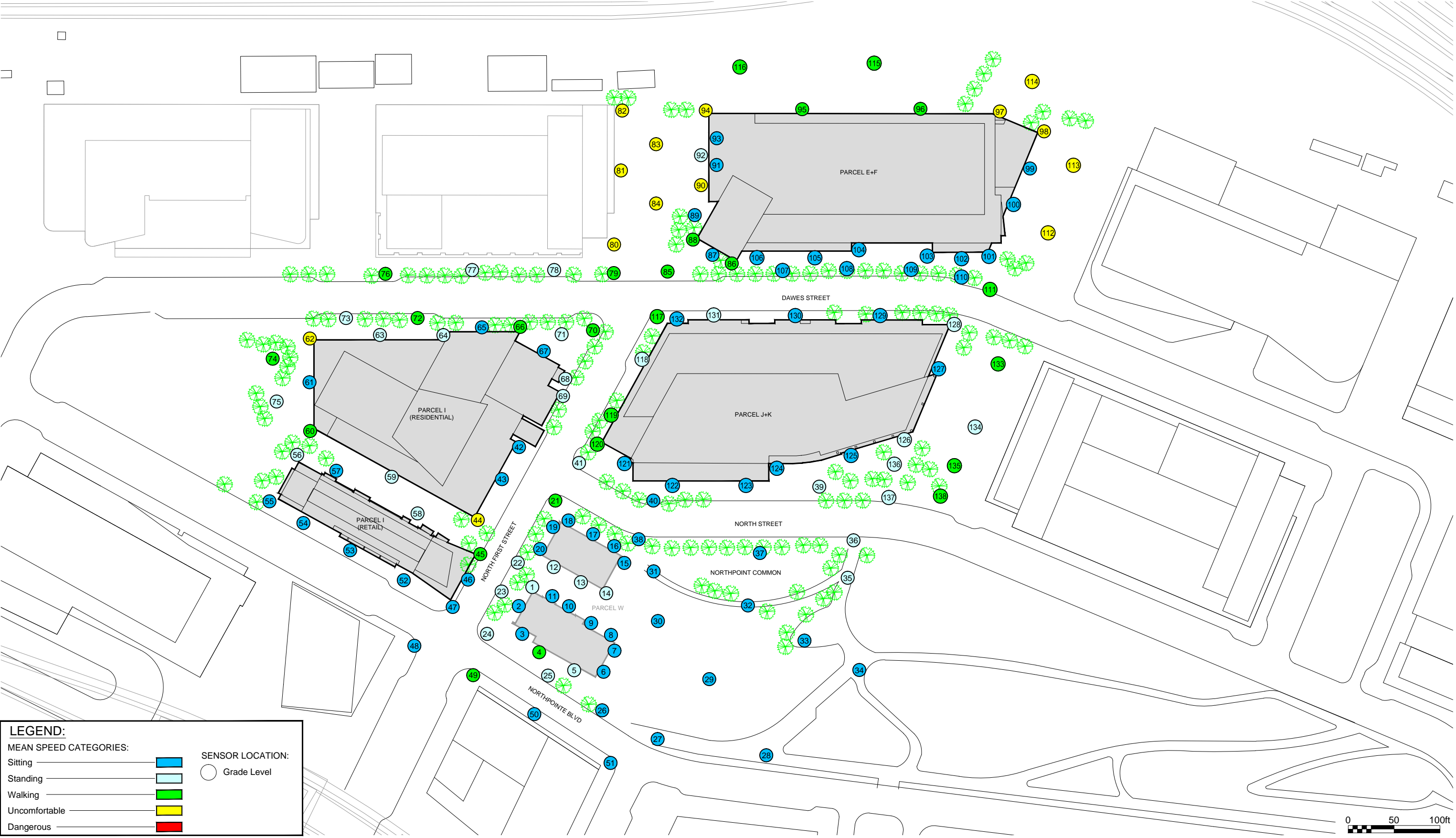


Pedestrian Wind Conditions - Mean Wind Speed - Full Build
Annual (January to December, 6:00 to 23:00) Comfort

Northpoint - Parcels W, I, E+F, and J+K - Cambridge, MA

True North

Drawn by: ck	Figure: 3C
Approx. Scale: 1"=100'	
Date Revised: Oct 5, 2016	



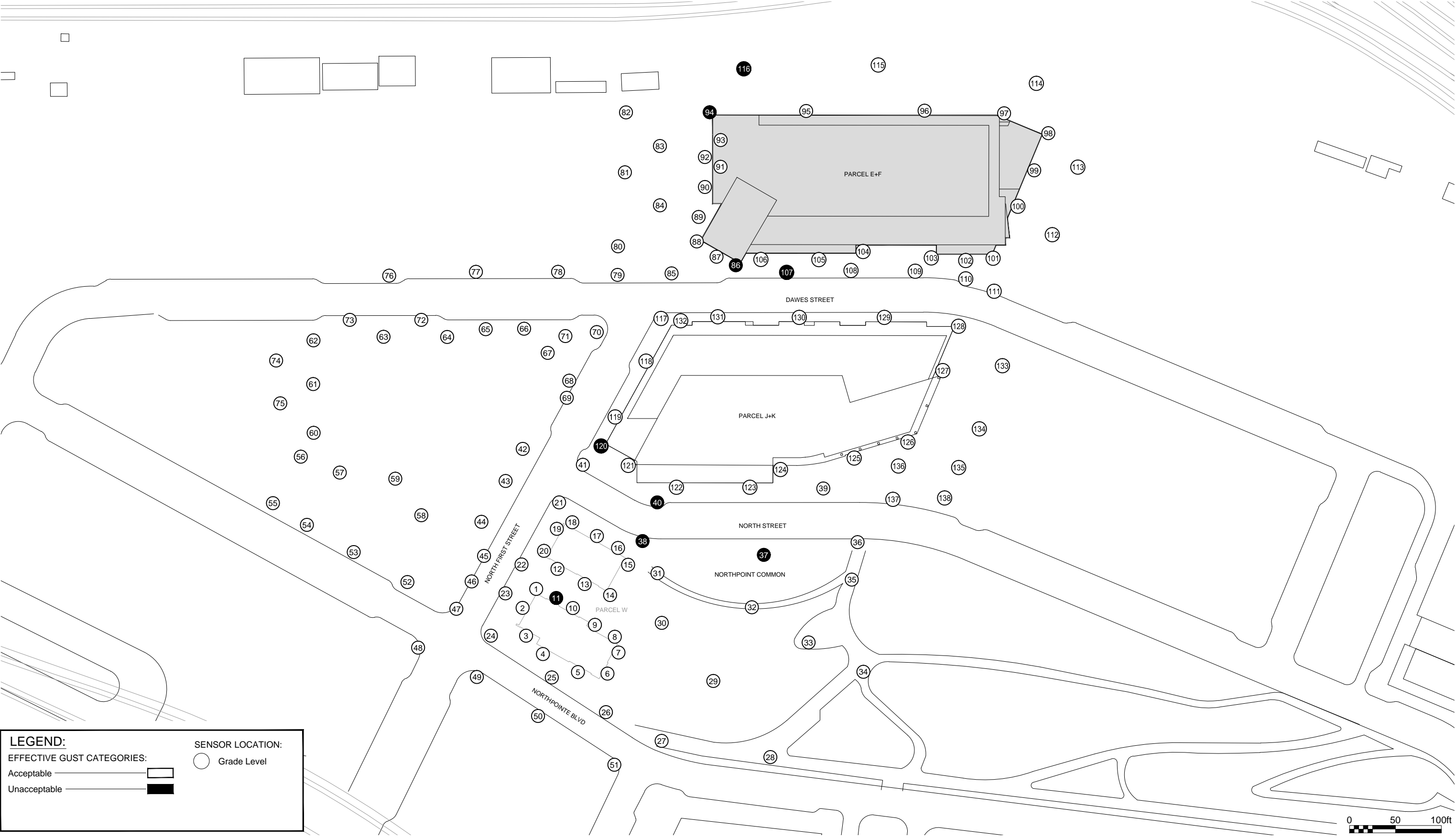
LEGEND:

MEAN SPEED CATEGORIES:

Sitting	Blue
Standing	Light Blue
Walking	Green
Uncomfortable	Yellow
Dangerous	Red

SENSOR LOCATION:

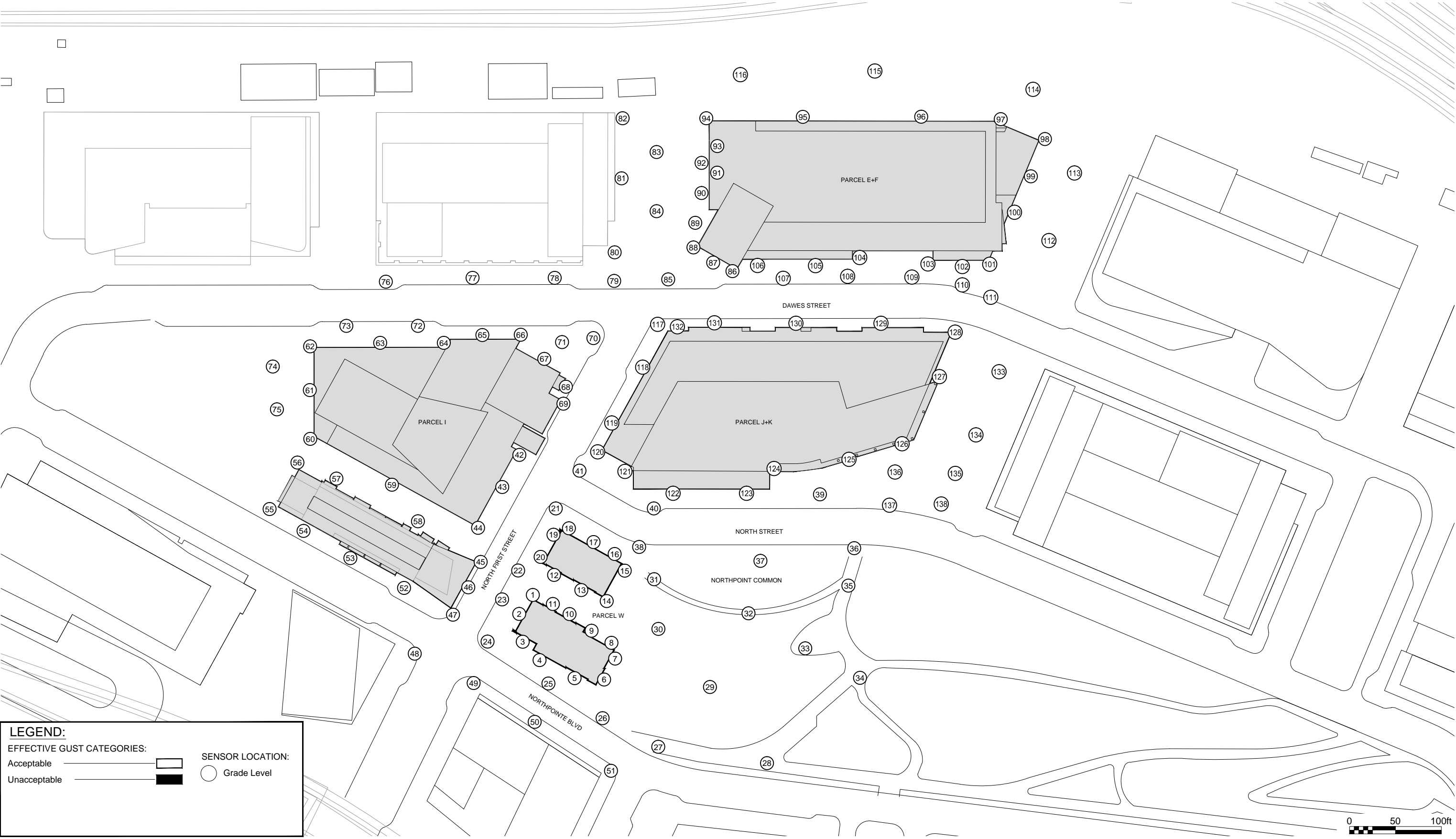
○	Grade Level
---	-------------



Pedestrian Wind Conditions - Effective Gust Speed - Parcels E+F
Annual (January to December, 0:00 to 23:00) Safety





Pedestrian Wind Conditions - Effective Gust Speed - Parcels E+F - With Mitigation
Annual (January to December, 0:00 to 23:00) Safety




LEGEND:

EFFECTIVE GUST CATEGORIES:

Acceptable 

Unacceptable 

SENSOR LOCATION:

 Grade Level



Pedestrian Wind Conditions - Effective Gust Speed - Full Build - With Mitigation
Annual (January to December, 0:00 to 23:00) Safety

Northpoint - Parcels W, I, E+F, and J+K - Cambridge, MA

True North

Drawn by: ck	Figure: 4d
Approx. Scale: 1"=100'	
Date Revised: Oct. 5, 2016	

RWDI
ARCHITECTS
PLANNERS
ENGINEERS

Project #1700383

MEMORANDUM

DATE:	2017-03-10	RWDI REFERENCE #: 1700936
TO:	Stephen Palumbo	EMAIL: spalumbo@tka-architects.com
FROM:	Michael Pieterse Michael Ratcliff Sonia Beaulieu	EMAIL: michael.pieterse@rwdi.com michael.ratcliff@rwdi.com sonia.beaulieu@rwdi.com
RE:	Building Air Quality Assessment – Design Review Northpoint Development – Parcels E+F Cambridge, MA	

Stephen,

RWDI has completed an initial review of the architectural and mechanicals plans for Parcels E+F of the Northpoint development. The purpose of this document is to provide our initial findings as they pertain to air quality at the proposed building. Parcels E+F are located along the north side of the development bordering train tracks to the north, Parcels C+D to the west, J+K to the south, and G to the east, as shown in Image 1 below.

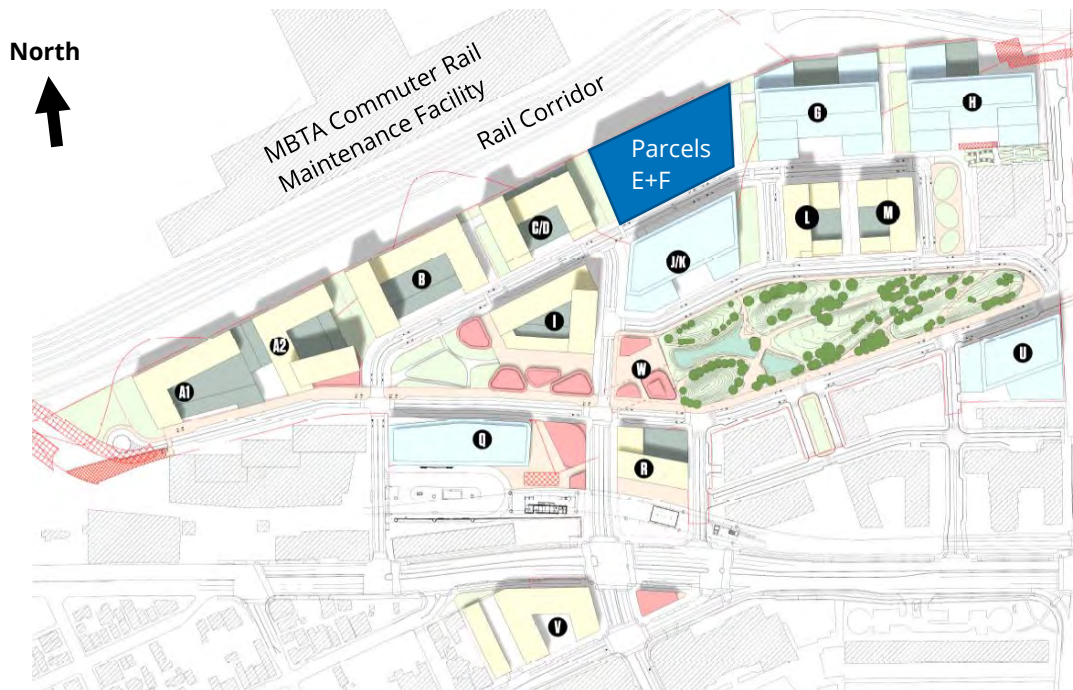


Image 1: Location of Parcels E+F of the Northpoint Development

The proposed building will be nine stories in height plus a mechanical penthouse. The new mechanical equipment that will be important from an air quality perspective is located in the lower level of the mechanical penthouse and on the roof, as shown in Images 2 and 3.

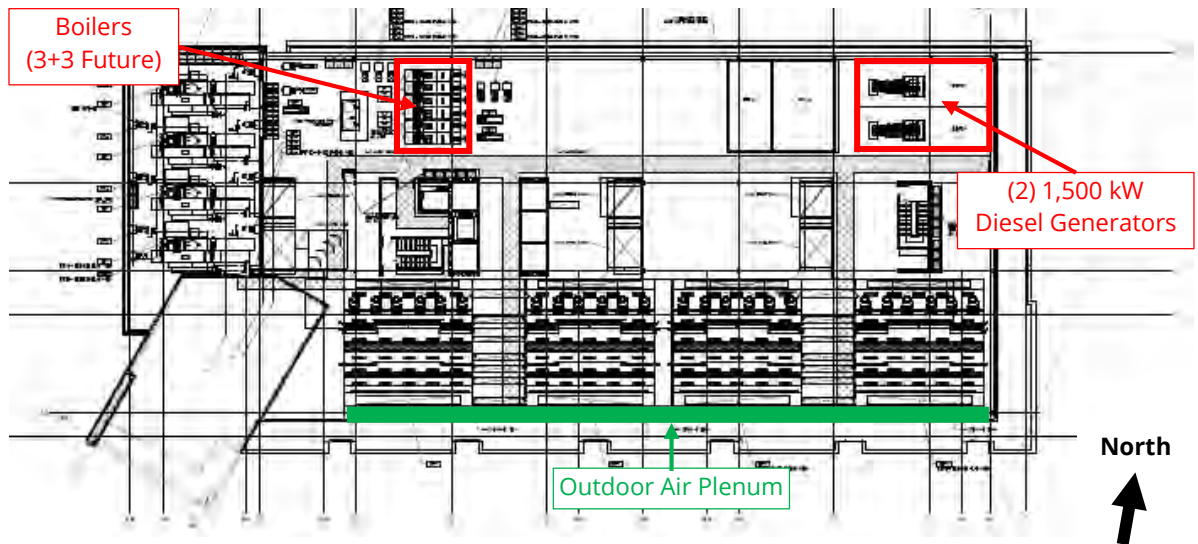


Image 2: Locations of Mechanical Equipment at Lower Penthouse Level

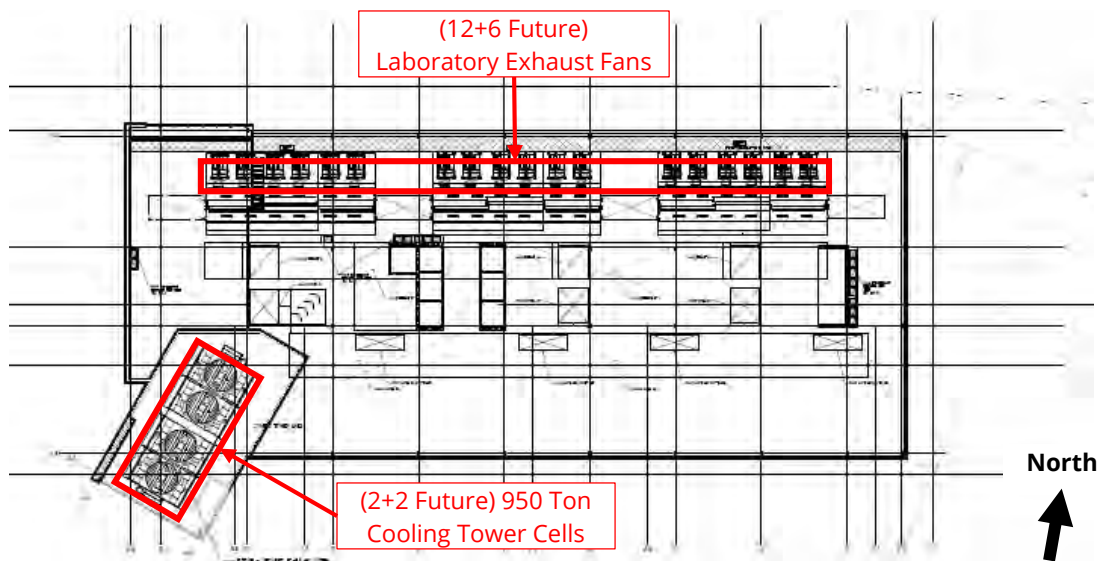


Image 3: Locations of Mechanical Equipment at Roof Level



DESIGN COMMENTS

Laboratory Exhausts

As the building will house research tenants, it is assumed that the exhaust air handling system will be connected to laboratory fume hoods. The total flow rate of exhaust air is anticipated to range from 168,000 cfm to 224,000 cfm. This volume will be connected to two or three large exhaust air handling units each with six fans. Review of the cut sheet for the proposed EAHUs indicate that the stacks will discharge 20 ft above the lower penthouse roof (5 ft above upper penthouse roof), and the discharge velocity will be at least 3,000 fpm. Based on this design, no significant concerns are anticipated at the proposed air intakes when the fans operate at full flow conditions. We are not aware of the location of air intakes on Parcels C+D, which is expected to be taller than E+F; it is possible that less dilution would be achieved at this building than we would recommend. If there are any air intakes on the east side of Parcels C+D, or there is a desire to operate the fans with reduced airflow volumes to save energy, we would recommend that wind tunnel testing be performed to quantify a safe lower limit to this flow. We recommend that the intakes on C+D be strategically placed to avoid impacts from Parcels E+F.

Diesel Generators

Two 1,500 kW diesel generators are located in the northeast corner of the penthouse. Having the exhaust stacks discharging vertically above the roof, without fixed rain caps, directly above the units would be positive as there would be good separation from the proposed air intakes. Since a large wake could form on the roof of the building during northerly winds, it will be important that detailed modelling be performed to identify an appropriate stack height to limit air quality impacts. Wind tunnel testing can also be used to predict the frequency of winds that would have the potential to cause odors at the air intake. In general, testing the generators during hours of low occupancy can be an effective strategy to reduce the potential for odor concerns.

Cooling Towers

The cooling towers are situated on the highest point of the building with the air intakes situated below, which is positive from a dispersion perspective. No significant air quality concerns are anticipated at Parcels E+F provided that low-odor/low-toxicity water treatment chemicals are used. This conclusion is predicated on not locating the intakes on Parcel C+D on the east façade, which could lead to serious impacts from the E+F towers. To prevent the spread of Legionella bacteria, we recommend that the towers be maintained according to manufacturer recommendations and practices outlined by ASHRAE¹ and the Cooling Tower Institute².

Boilers

Three to six boilers are anticipated for the penthouse. Discharging the flues vertically above the roof without fixed rain caps will result in higher levels of dispersion as compared to other flue configurations (e.g., discharging

¹ ASHRAE. Guideline 12-2000. Minimizing the Risk of Legionellosis Associated with Building Water Systems. American Society of Heating, Refrigerating and Air-Conditioning Engineers, Inc., Atlanta, Georgia

² Cooling Technology Institute. July 2008. Legionellosis Guideline: Best Practices for Control of Legionella.



horizontally, or from a vertical flue with a rain cap). Provided that the flue discharges vertically, are separated from the air intake by at least 50 ft and the units are low-NO_x natural gas boilers, no significant concerns due to pollutant levels would be anticipated.

Rail Corridor

Parcels E+F back onto a rail corridor where it is assumed that diesel locomotives travel. A Massachusetts Bay Transportation Authority (MBTA) Commuter Rail Maintenance Facility is located just to the north of this site. The MBTA operates a fleet of diesel locomotives and utilizes this rail corridor for regular commuter rail service. Therefore, it is assumed that there will be regular and frequent diesel locomotive traffic to the north of Parcels E+F.

The air intakes on the south façade of the proposed building are well situated to increase separation distance and prevent “line-of-sight” between the intakes and locomotives. This protection will improve the level of dilution between the engine exhaust before reaching the air intakes. While the engine exhaust is expected to be sufficiently diluted such that no significant health-related concerns are expected, it is possible that detectable levels of odor could reach the fresh air intakes during northerly winds. It is recommended that wind tunnel modeling be performed to quantify the risk for odors from locomotives and determine whether the air intakes should be treated using activated carbon filters to prevent nuisance odors from entering the building.

Sources on Parcels J+K

The highest architectural elevation of the Parcel E+F building is 188 ft above grade. Based on our previous work on Parcels J+K, we understand that that building will be of a similar height, and will feature laboratory exhausts and a diesel generator discharging above roof level. The Parcel J+K laboratory exhausts are not expected to present a significant concern at Parcels E+F owing to the high flow rate which offers significant levels of dilution.

The Parcel J+K generator is not a concern for the proposed air intakes due to pollutant levels; detectable levels of odors could enter Parcel E+F when the generator operates during southerly winds. Given the infrequency of generator operation (i.e., scheduled testing and during loss of power), this may not be a significant concern at Parcel E+F.

Sources On Other Parcels

We are not aware of the specific exhausts on any parcels other than J+K. The roof of the adjacent Parcels C+D is expected to be approximately 50 ft higher than that of Parcels E+F. As a result, the exhausts on the roof of this parcel are unlikely to be a significant concern at Parcels E+F. Parcel G is expected to be approximately 10 to 20 ft lower in height than Parcels E+F, however with the fresh air intakes on the south façade, sources on this roof are unlikely to be a significant concern.



CLOSING

We would like to discuss this report and obtain your feedback via conference call at your earliest opportunity. If you have any questions, please do not hesitate to contact us.

Sincerely,

RWDI Inc.

Michael Pieterse, M.A.Sc., P.Eng.
Project Engineer

Michael Ratcliff, Ph.D., PE
Technical Director

Sonia Beaulieu, M.Sc., P.Eng., ing.
Senior Project Manager / Principal

MEMORANDUM

Date: September 21, 2016

Prepared for: Beals & Thomas, Inc.
144 Turnpike Road
Southborough, MA 01772

Prepared by: Rebecca Snelling, Project Manager
Jesse Freeman, EIT, Senior Project Manager
The VERTEX Companies, Inc.

Subject: **21E Reports and Environmental Assessments**
Parcel J/K
Northpoint Development Site
Cambridge, Somerville, & Boston, Massachusetts
VERTEX Project No. 35663

The Vertex Companies, Inc. (VERTEX) has prepared this brief summary of the environmental status, environmental reports, and assessments pertaining to the portion of the Northpoint Development Site inclusive of Parcel J/K.

Currently the only active Release Tracking Number (RTN) tracked by the Massachusetts Department of Environmental Protection (MassDEP) pertaining to Parcel J/K is RTN 3-11533. The release listed under RTN 3-11533 is comprised of the linked RTNs 3-12277, 3-26810, 3-29887, and 3-32730, which were closed following linking to RTN 3-11533 and are henceforth managed under the master RTN 3-11533. The linked release listed under RTN 3-11533 is located on portions of the former Boston & Maine (B&M) Railroad Yard currently identified as the Northpoint Development Site located in portions of Cambridge, Somerville, and Boston, Massachusetts (the "Site"), a portion of which includes Parcel J/K.

The linked releases listed under RTN 3-11533 pertains to elevated concentrations of contaminants of concern (COCs) in soil associated with the historical industrial/commercial uses of the Site and the widespread presence of urban fill materials.

A Class C-1 Response Action Outcome (RAO) was submitted for RTN 3-11533/3-12277 on behalf of then owner B&M on July 1, 2005, and a Phase II Comprehensive Site Assessment (CSA), Phase III Remedial Action Plan (RAP), and Class C-2 RAO report (Phase II/Phase III/Class C-2 RAO) was submitted for RTN 3-26810 on behalf of CJUF on September 26, 2012.

RTN 3-26810 was linked to RTN 3-11533 on June 1, 2015 in a Post-Temporary Solution Status Report, 5-Year Periodic Review, & Request to Link Release Tracking Numbers report.



Following linking, Master Site RTN 3-11533 is managed by current Site owner DW NP Property, LLC (DW) under a single Temporary Solution. DW is an Eligible Person for RTN 3-11533 per Massachusetts General Laws (M.G.L) chapter (c.) 21E.

Details regarding the regulatory status and environmental investigations conducted to date on the portion of the Site currently referred to as Parcel J/K are included in various site-wide reports prepared from 1995 through the date of this letter, as listed below. Reports specific to Parcel J/K have not been prepared.

RTN 3-11533/3-12277

- October 6, 1995 – RTN 3-11533/3-12277 – Phase I Initial Site Investigation (ISI) Report
- February 26, 1999 – RTN 3-11533/3-12277 – Phase II Comprehensive Site Assessment (CSA)
- February 26, 1999 – RTN 3-11533/3-12277 – Phase III Remedial Action Plan (RAP)
- July 2005 – RTN 3-11533/3-12277 – Class C RAO¹ (now Temporary Solution)
- December 1, 2005 – RTN 3-11533/3-12277 – Additional Class C RAO Info Report
- April 14, 2011 – RTN 3-11533/3-12277 – Class C RAO 5-Year Periodic Review

RTN 3-26810 (and linked RTN 3-29887)

- May 5, 2008 – RTN 3-26810 – MCP Phase I ISI & Tier Classification – NP “Gateway” Parcels
- September 26, 2012 – RTN 3-26810 – Phase II CSA, Phase III RAP, and Class C RAO
- December 22, 2014 – RTN 3-26810 – Post-Temporary Solution Status Report No. 1

Site-wide Due Diligence – Not RTN Related

- November 3, 2009 – ASTM Phase I Environmental Site Assessment (ESA) – Site-wide
- March 13, 2015 – ASTM Phase I ESA – NP Parcel 1

RTN 3-11533 (et al. following linking, RTN 3-12277, 3-26810, 3-29887, 3-32370)

- June 1, 2015 – RTN 3-11533 (et al.) – Post-Temporary Solution Status Report No. 1, 5-Year Periodic Review, and Request to Link Release Tracking Numbers

¹ In accordance with the promulgation of the new MCP requirements in effect June 20, 2014, as contained in Section 310 CMR 40.1055 of the MCP, any Class C RAO submitted prior to June 20, 2014 shall now be a Temporary Solution as described in 310 CMR 40.1050(1)(e).

- November 16, 2015 – RTN 3-11533 (et al.) – Site Staging Release Abatement Measure (RAM) Plan
- December 7, 2015 – RTN 3-11533 (et al.) – Post-Temporary Solution Status Report No. 2
- March 22, 2016 – RTN 3-11533 (et al.) – Site Staging RAM Status Report No. 1
- April 28, 2016 – RTN 3-11533 (et al.) – Site-wide RAM Modification
- July 7, 2016 – RTN 3-11533 (et al.) – Post Temporary Solution Status Report No. 3
- September 21, 2016 – RTN 3-11533 (et al.) – Site-wide RAM Status Report No. 2

Currently, the regulatory management of the release at Parcel J/K is being conducted under the RTN 3-11533 Temporary Solution and under the RTN 3-11533 Site-wide RAM.

Copies of all relevant reports and the Disposal Site files for all above-referenced RTNs can be reviewed at Massachusetts Department of Environmental Protection (MassDEP), Northeast Regional Office, 205B Lowell Street, Wilmington, MA, (978) 694-3200 or online at the MassDEP Waste Site File Viewer at http://public.dep.state.ma.us/wsc_viewer/main.aspx by searching individual RTNs.

Additionally, there has been no evidence to date of underground storage tanks (USTs) being present at Parcel J/K.

The name and address of the party conducting response actions at the Disposal Site and the Licensed Site Professional (LSP) for the Disposal Site are as follow:

Party Conducting Response Actions: DW NP Property, LLC, an Eligible Person
c/o Mark Johnson²
Director of Development
DivcoWest Real Estate Investments
1 Kendall Square, Suite B3201
Cambridge, MA 02139

Licensed Site Professional: James B. O'Brien, LSP # 9092
The Vertex Companies, Inc.
400 Libbey Parkway
Weymouth, MA 02189
781-952-6000

² Note that Mark Johnson is electronically signing eDEP Bureau of Waste Site Cleanup (BWSC) Transmittal Forms on behalf of DW NP Property, LLC and not individually.

Northpoint Parcel JK

Construction and Site Activities Summary

Submitted to:
City of Somerville Planning Board

Submitted by:
Consigli Construction Company
72 Sumner Street, Milford MA 01757
November 9, 2016

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- A) Responsible Parties
- B) Project description
- C) Schedule
- D) Phases of construction
- E) Detailed Information
 - 1) Work hours
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 - 18) Summary

A) Responsible Parties:

Owner:

DWNP LLC
c/o DivcoWest Real Estate Investments
575 Market Street
San Francisco, California 94105
415-284-5700

Development Manager:

Mark Johnson, Director of Development
DivcoWest
One Kendall Square, Suite B3201
Cambridge, MA 02139
617-720-7400

Contractor:

Jeff Navin, Project Executive
Consigli Construction Company
72 Sumner Street
Milford, MA 01757
508.458.0329

Architect:

Richard L. Kobus, Senior Principal
Tsoi Kobus & Associates
One Brattle Square, Cambridge, MA 02238
617-475-4000

Structural Engineer:

Mysore Ravindra,
Principal
LeMessurier
1380 Soldiers Field Road,
Boston, MA 02135
Phone: 617.868.1200

Mechanical, Electrical, Plumbing, and Fire Protection Engineer:

Allan Ames,
Principal
BR+A
10 Guest Street, 4th Flr
Boston, MA 02135
Phone: 617.254.0016

Civil Engineer:

Richard Kosian, Executive Vice President
Beals + Thomas
144 Turnpike Road
Southborough, MA 01772
508-366-0560

Geotechnical Engineer:

Steven Kraemer, PE Senior Vice President
Haley & Aldrich
465 Medford Street, Suite 2200
Boston, MA 02129
617-886-7350

B.) Project Description:

The Project is new construction 635,372 GSF, nine story building. The project includes three levels of below grade parking, ground floor retail/ Active use, tenant space, bike storage and locker shower rooms and back of house areas including loading docks and mechanical and electrical rooms. Floors 2 through 9 are shell - core laboratory and office space. A mechanical penthouse on the roof includes air handling units, chillers, cooling tower, pumps, boilers and emergency generator.

C.) Schedule: Total Duration 28 Months

Assumed Construction Start: May, 2017

Assumed Completion and Occupancy: November, 2019

<u>Construction Activity</u>	<u>Approximate Schedule</u>	<u>Duration</u>
SOE/Excavation	Months 0 -8	8 months
Foundation	Months 8 - 13	5 Months
Structure	Months 11 - 15	5 Months
Exterior Envelope	Months 14 -21	7 Months
MEP Rough	Months 14-19	13 Months
Elevators	Months 16-26	10 Months
Interiors	Months 15-26	16 Months
Site Utilities	Months 14 -18	4 Months
Site Improvements	Months 24 – 28	5 Months

D.) Phases of Construction

SOE/Excavation work includes the mobilization, site and utility prep as well as the SOE, cross lot bracing and the excavation.

Foundation work includes the mat slab for the cores as well as the concrete footings, core walls and the perimeter walls of the basement.

Structural work includes the structural steel erection starting at the basement footings through the penthouse including deck and detail of the SOMD. Two tower cranes will be used for steel erection including metal deck, screen wall, and rooftop HVAC equipment.

Exterior work includes the installation of sheathing, insulation, curtainwall, glass and metal

panels and roofs. (entire envelope)

Elevator, MEP and finish work will complete the interior fit out of the building. Most of the finish materials will be loaded into the building via a temporary loading dock and a temporary man and materials hoist installed on the East face of the building.

Site utilities work will include the installation of gas, electric, tel/data, sewer and storm water connections.

Site improvement work includes the installation of sidewalks, site lighting, site drainage and landscaping.

E.) Detailed information:

1. Work Hours: Construction work hours are 7:00 AM to 6:00 PM Monday thru Friday. 7:00 AM to 5:00 PM on Saturday. Utility and special operations (i.e. tower crane and special installations) will be on 2nd shift or weekends. All street occupancies will be scheduled and permitted with the proper city agency having jurisdiction. Extended work hours as necessary will be through the approved permit process.
2. Delivery and Trucking Routes. Construction traffic will vary depending on the various stages of construction activity. Truck access to and from the site shall be limited to the attached plan and per the City of Cambridge restricted trucking routes. Trucking in and out of the site will be directed toward Msgr O'Brien Highway and over the Gilmore Bridge towards Route 93. All subcontractors will be required to monitor and confirm that restricted truck routes are not being used. Sequencing of deliveries will be planned during weekly coordination meetings to minimize disruptions to traffic. Deliveries will be scheduled to avoid rush hour traffic to the greatest extent possible.
3. Worker Access and Parking. The number of workers required for the project varies for each phase. Approximately 20 to 25 workers will be on site during the foundation phase and the work force will increase to possibly 150 – 200 workers during the peak construction period. There will be no parking on any public roads abutting the site or adjacent neighborhoods. All workers will be encouraged to use public transportation and carpooling. Public transportation information will be posted on site. Consigli will also utilize a fenced off adjacent vacant lot for construction trailers and limited worker and supervisory staff parking. In addition, jobsite tool storage boxes will be utilized to encourage workmen to commute by public transportation without transporting tools and equipment on a daily basis.
4. Police Details. Daily police details will be provided as needed at each active construction gate to maintain access to adjacent properties and to direct pedestrian and vehicular flow.

5. Truck unloading and staging. There will be no truck staging on local public roads at all. Consigli will make every effort to stage trucks on site. Per the attached plan, the steel delivery trucks will be staged on site inside the fence off of Dawes Street. Offloading of trucks will not be permitted on city streets. The steel supplier will be responsible to coordinate timely deliveries and staging area off site if necessary. Concrete trucks will be dispatched by radio using the construction site for staging. The exterior hoist and temporary loading dock will be completed as soon as possible so most materials for interior construction will be unloaded inside the site at the temporary loading dock. Deliveries will be coordinated at the regularly scheduled foreman's meeting.
6. Construction site signs. Signage will be provided at all gates and closed sidewalks directing the pedestrians around the site. Trucking enter, exit and turn signs will be posted at gates and strategic locations on Msgr O'Brien Highway and elsewhere around the project site directing trucks out of the City. Utility work will have the proper signage for lane restrictions or road closures as required by the permit.
7. On Street Parking. There will be no parking on the perimeter streets or neighborhoods. Parking at meters will be discouraged. All workers will be encouraged to use public transportation and carpooling or public garages.
8. Pedestrian Access. Public safety will be a primary consideration in all the planning and building processes. We do not anticipate closed sidewalks, but if needed proposed pedestrian paths will be clearly marked with signage. Bike lanes will remain open.
9. Dust control. A mechanical street sweeper shall clean the adjacent streets and proposed trucking routes full time during excavation and concrete operations. Wheel wash stations will be maintained for gates that are used for trucks exiting the site. Water spray will be used to control dust during precast pile, concrete and site operations. All trucks transporting construction debris and materials will be tarped and wheels cleaned before leaving site. No storage of construction debris will be allowed on site. Streets and sidewalks will be cleaned periodically to minimize dirt and dust accumulations.
10. Odor control. Consigli will make every attempt to control nuisance odor emissions by controlling site drainage to minimize standing water, covering stockpiles, adding masking agents to address offensive smells and maintaining a site free of trash, garbage and debris. Consigli will also control idling of trucks and construction equipment and try to keep combustion engines away from air intakes, air conditioners and windows.
11. Rodent control. Consigli will institute a rodent / pest control program and will hire a rodent control subcontractor to service the site on a regular basis. Adjustments will be made to the program as necessary to maintain a rodent / pest free site.
12. Emergency vehicles. Access for emergency vehicles will be maintained at all times on Msgr O'Brien Highway, East Street and North Point Blvd. Existing fire hydrants located

outside of site fence will be clearly labeled with signage readily visible for Fire Department personnel in the event of an emergency.

13. Utilities. Utility work, lane restrictions, impacts to the neighbors and traffic will be coordinated with the utility providers, the City of Cambridge DPW, the City of Somerville DPW and other contractors working in the area.
14. Snow removal. Snow accumulation on abutting pedestrian walkways/sidewalks will be removed and deposited on site or disposed of properly.
15. Site dewatering. Dewatering will be from sumps into Frac tanks for filtration and then discharged on site. Required permits from the MWRA, DEP and DPW will be obtained for the offsite discharge of water.
16. Construction waste. This project is seeking a LEED Silver certification and a goal of 75% recycled waste. The disposal contract will include specific requirements that will ensure the segregation, reprocessing and recycling of the construction waste. City of Cambridge dumpster permits will be on site.
17. Vibrations. Consigli will work with DivcoWest and soils Engineer to monitor and mitigate any potential impact the construction activities may have on abutting neighbors and nearby residences throughout the foundation installation. Vibration monitoring stations will be utilized.
18. Summary: Consigli Construction Company will work with the City of Somerville, DPW, ISD, Transportation Department and pertinent City groups to ensure a safe and effective program to protect and coordinate the interest of the neighborhood and the City of Somerville.



Memorandum

To: Mr. Kevin Prior, Chair
Somerville Planning Board
City Hall
93 Highland Avenue
Somerville, MA 02143

Date: November 17, 2016

Project #: 13369.00

From: Susan Sloan-Rossiter, Principal
Meghan Houdlette, P.E.
VHB

Re: NorthPoint Transportation Analysis for Parcel E/F

Introduction

The NorthPoint Development, located in Somerville, Cambridge, and Boston Massachusetts along the northeast side of O'Brien Highway has a long history of transportation planning and engineering with permitting documents spanning back to 2002. A Transportation Impact Study (TIS) for the NorthPoint project was submitted to the City of Cambridge as part of the Special Permit process which was certified by the City in 2002. Additionally, a Neighborhood Development Plan Transportation Memo was submitted to the City of Somerville on September 28, 2016. The Parcel E/F Design and Site Plan Review (DSR) Application is consistent with the program and transportation analysis presented in the updated and approved Somerville Neighborhood Development Plan as described below.

Parcel E/F Program

The DSR application proposes a 375,000 sf lab building with 25,000 sf of ground floor retail on Parcel E/F as shown in Table 1. The building will be supported by 379 parking spaces.

Table 1 - NorthPoint Program Parcel E/F

Program	2016 Master Plan Program (GSF)
Lab	375,000
<u>Retail</u>	<u>25,000</u>
Total	400,000

Consistency with Approved Development Program for NorthPoint

Table 2 compares the original NorthPoint development program studied as part of the certified TIS to the 2016 approved NorthPoint Master Plan presented in the Somerville Neighborhood Development Plan. Three NorthPoint residential buildings have been constructed to date totaling 739,992 sf of residential and 8,600 sf of retail. The 2016 approved NorthPoint Master Plan includes a total of 5,245,854 sf of mixed use, therefore, the proposed Parcel E/F at 400,000 sf falls within the remaining program of 4,497,262 sf.

Table 2 – Full Build Program Comparison

Full Build Program	2002 TIS (GSF)	2016 Master Plan Program (GSF)	Constructed to Date¹	Remaining to Build
Commercial Office, R&D, Lab ²	2,140,000	1,785,000	0	1,785,000
Retail and Consumer Service	75,000	300,000	8,600	291,400
Lodging	90,000 (90 keys)	100,062 (100 keys)	0	100,062 (100 keys)
<u>Residential</u>	<u>3,235,000</u> <u>(2,790 units)</u>	<u>3,060,792 (3,177 units)</u>	<u>739,992</u>	<u>2,320,800</u>
Total	5,540,000	5,245,854	748,592	4,497,262

¹ Residential Buildings on Parcels S, T, and N have been constructed as of October, 2016

² Commercial Office, R&D and/or Laboratory or a combination of these uses

Trip Generation Analysis

Table 3 presents the peak hour vehicle trips for the Full Build Program under the original TIS, the approved 2016 Master Plan and what has been constructed to date. Parcel E/F is expected to generate approximately 176 and 222 vehicle trips during the morning and evening peak hour which is within the permitted remaining 1,324 and 1,624 vehicle trips during the morning and evening peak hours respectively. A more detailed description of the trip generation methodology is presented in the attached Somerville Neighborhood Development Plan Transportation Memo.

Table 3 - Comparison of Vehicle Trip Generation

<i>Full Build</i>	Daily	Total Vehicle-Trips					
		AM			PM		
		In	Out	Total	In	Out	Total
2002 TIS	16,013	944	751	1,695	736	1,105	1,841
2016 Master Plan	15,980	873	563	1,436	697	1,053	1,750
Constructed to Date ¹	1,375	25	87	112	80	46	126
Remaining Trips (Master Plan - Constructed to Date)	14,605	848	476	1,324	617	1,007	1,624
Parcel E/F	1,830	145	31	176	62	160	222

¹ Residential Buildings on Parcels S, T, and N have been constructed as of October, 2016

Conclusion

As discussed, the results of the trip generation analysis demonstrate that the 2016 Master Plan development program does not exceed the trip generation threshold of 16,013 daily vehicle trips, 1,695 AM peak hour vehicle trips and 1,841 PM peak hour vehicle trips as presented in the NorthPoint TIS certified by the City of Cambridge Traffic and Parking Department dated November 14, 2002. The proposed Parcel E/F is consistent with the approved 2016 Master Plan program. Parcel E/F is expected to generate approximately 1,830 daily vehicle trips, 176 total morning peak hour vehicle trips and 222 total evening peak hour vehicle trips which is consistent with the permitted threshold for the entire development.



HALEY & ALDRICH, INC.
465 Medford St.
Suite 2200
Boston, MA 02129
617.886.7400

9 November 2016
File No. 05304-560

DIVCO West
One Kendall Square
Suite B3201
Cambridge, Massachusetts 02139

Attention: Mark Johnson
Director of Development

Copy: John Weigel, John Rappoport

Subject: City of Somerville – Geotechnical Items
North Point Parcel EF
Somerville, Massachusetts

Dear Mr. Johnson:

We have prepared this letter to address two geotechnical items raised by the City of Somerville related to the construction of the proposed North Point building at Parcel EF. Our comments below are based on our understanding of the current Parcel EF building and site design, anticipated construction schedule relative to overall site infrastructure and Parcel JK construction, as well as our recent geotechnical design investigation, which included a comprehensive field and laboratory investigation program. The subsurface investigation and design recommendations will be summarized in a forthcoming geotechnical report.

SITE AND SUBSURFACE CONDITIONS

Subsurface investigations at Parcel EF indicate the following subsurface strata, listed in order of occurrence below ground surface

STRATUM	THICKNESS (ft)
Miscellaneous Fill	15 to 28.5
Organic Deposits	0 to 12
Marine Clay Deposits	19 to 51
Glacial Deposits	7 to 20
Bedrock (Argillite)	Top of Bedrock 68 to 86 ft below ground surface

Based on groundwater observation well data and field observations, groundwater levels typically range between about El. 13 to El. 15 (Cambridge City Base). Site groundwater levels fluctuate with precipitation and other factors, and can be perched at shallow depths in the fill soils during wet seasonal periods.

A roadway and infrastructure surcharge program is currently being conducted to mitigate post-construction settlements of utilities, adjacent streets and sidewalks, in preparation for site-wide roadway and utility construction for the overall North Point development. The surcharging involves installation and monitoring of temporary earth embankments within the road rights-of-way. Surcharging is being performed now on Dawes Street to the south of the Parcel EF site. Additional surcharging is planned in the future for parks and driveways to the east and west of the Parcel EF building; however, the timing relative to the Parcel EF construction is uncertain.

The Parcel EF site consists of an open lot, with current grades in and around the area ranging from about El. 15 in the middle of the site, up as high as El. 35 at the top of additional fill placed for surcharge embankments. Final surrounding grades and the corresponding EF building ground floor are anticipated to range from El. 29.5 to El. 32.

There are currently no existing adjacent structures or known active utilities in and around the Parcel EF site; some metal storage containers exist to the north of the north Parcel property line, on the adjacent MBTA property. The nearest existing MBTA rail track is at least 85 ft away from the property line, and over 100 ft from the planned building construction work.

PROPOSED BELOW-GRADE CONSTRUCTION AT PARCEL EF

The proposed Parcel EF building, which is designed to be supported on end-bearing precast pre-stressed concrete pile foundations, includes a 3-level below-grade parking garage. Excavation for the parking garage will require an approximately 35 to 40-ft deep excavation over most of the site. A lateral excavation support system consisting of steel sheet piles with multiple levels of internal bracing will be used to limit the extent of the excavation and construct the below-grade space.

SCHEDULE

Parcel EF building construction and site-wide infrastructure work are planned to commence in 2017. Support of excavation (SOE) installation, pile driving and mass excavation for the EF building are planned to begin in the late second to third quarter; site-wide utility and roadway installations will begin in the second quarter and be completed in 2017.

GEOTECHNICAL ITEMS RAISED BY CITY

The City of Somerville has requested information on the following two geotechnical items related to the proposed building construction:

- “Analysis of sub-soil conditions, the potential for ground movement and settlement during construction, and the impact on adjacent buildings and utility lines and a description of foundation construction methodology”
- “Analysis of the impact of construction on groundwater levels and resulting effects on surrounding structures and building foundations”

As the issues involved in these two items are very intertwined, they are addressed together below.

As part of the Haley & Aldrich geotechnical design investigation, the potential for ground movements due to the excavation and issues related to dewatering and groundwater impacts were evaluated extensively. The

lateral excavation support system will utilize a watertight, interlocking steel sheet pile wall installed deep into the soils below the bottom of the excavation, to form a continuous groundwater cutoff. This wall, to be seated entirely in low permeability clay soils, will enable dewatering inside the excavation without impacting groundwater levels outside the excavation. Accordingly, the dewatering is not anticipated to induce ground settlements. Further, as there are no existing structures or other permanent facilities near the EF site, it is anticipated that there will be no significant adverse effects of the dewatering.

Haley & Aldrich has evaluated the anticipated performance of the excavation support system and driven pile foundation system, including potential ground movements around the excavation. Small lateral movements of the sheeting and soil immediately around the excavation are anticipated to occur, on the order of 1 to 2 inches. The lateral soil movements, coupled with localized effects of the pile driving, could result in small ground settlements of similar magnitude immediately around the excavation, dissipating quickly with distance away. As there will be no structures or significant facilities close to the Parcel EF site, adverse impacts due to potential ground movements are not anticipated.

A temporary electric service is planned to be installed along the north side of Dawes Street in early 2017, including a small substation about 60 ft away from the Parcel EF excavation and a below-grade duct bank about 20 ft away from the proposed building. As the below-grade duct bank is concrete-encased and relatively insensitive to minor settlements, adverse impacts on the temporary electric service are not anticipated.

Ground vibrations will be generated due to installation of sheeting and foundation piles. Based on experience in similar situations, the vibrations are not anticipated to create adverse impacts to the temporary electric service or new utilities if they are installed beneath the nearby streets prior to pile driving.

The performance of the excavation support system will be monitored during construction to confirm the anticipated behavior. Elevation reference points will be established and monitored on the temporary electric substation. Other site utilities may be installed along adjacent roadways concurrent with the basement excavation. Utilities having surface manifestations, such as manholes, will also be monitored if they are installed prior to the EF excavation being substantially completed. Vibration levels will be monitored during initial sheeting and pile installation, to confirm anticipated vibration magnitudes.

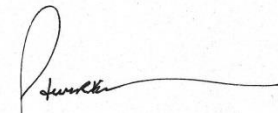
CLOSURE

In summary, the planned below-grade construction at North Point Parcel EF is not anticipated to have adverse impacts on groundwater levels, structures or other facilities.

Sincerely yours,
HALEY & ALDRICH, INC.



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